

**DISTRICT OF WEST VANCOUVER**  
750 17TH STREET, WEST VANCOUVER BC V7V 3T3

## COUNCIL REPORT

Date:	November 2, 2022
From:	Erik Wilhelm, Senior Community Planner
Subject:	Proposed Zoning Amendment, OCP Amendment and Development Permit for 671, 685, 693 Clyde Avenue and 694 Duchess Avenue
File:	05.1010.20/21-185.2022

### RECOMMENDATION

THAT opportunities for consultation on the proposed Official Community Plan amendment, with persons, organizations, and authorities, as outlined in the report dated November 2, 2022, be endorsed as sufficient consultation for purposes of section 475 of the *Local Government Act*.

### RECOMMENDATION

THAT proposed “Official Community Plan Bylaw No. 4985, 2018, Amendment Bylaw No. 5222, 2022” be read a first time.

### RECOMMENDATION

THAT proposed “Zoning Bylaw No. 4662, 2010, Amendment Bylaw No. 5223, 2022” be read a first time.

### RECOMMENDATION

THAT proposed “Official Community Plan Bylaw No. 4985, 2018, Amendment Bylaw No. 5222, 2022” and proposed “Zoning Bylaw No. 4662, 2010, Amendment Bylaw No. 5223, 2022” be presented at a public hearing on January 23, 2022, at 6 p.m. in the Municipal Hall Council Chamber and via electronic communication facilities (WebEx video conferencing software), and that notice be given of the scheduled public hearing.

### RECOMMENDATION

THAT proposed “Development Permit No. 21-185” be presented at a public meeting scheduled for January 23, 2022, at 6 p.m. in the Municipal Hall, to be held concurrently with the public hearing scheduled for January 23, 2022, at 6:00 p.m. in the Municipal Hall, and that notice be given of the scheduled public meeting.

## 1.0 Purpose

To present to Council a proposed development application to rezone 671, 685, 693 Clyde Avenue and 694 Duchess Avenue (**Appendix A**) to allow for a 201 unit market rental apartment development (See “Project Profile” – **Appendix B**) and a consequential text amendment to the Comprehensive Development Zone 30 applicable to Park Royal North. Presented as part of the development are:

- bylaws serving to amend the Official Community Plan and Zoning Bylaw; and
- a development permit to regulate the form and character of the development proposal.

## 2.0 Legislation/Bylaw/Policy

### *Provincial Legislation*

The *Local Government Act* requires that a Public Hearing be held on the proposed rezoning bylaw and OCP amendment bylaw, in accordance with Sections 464 through 470.

### *Transportation Act*

As the subject site is located within a “controlled area<sup>1</sup>”, in compliance with Section 52 of the Transportation Act, the Ministry of Transportation and Infrastructure (MOTI) will be required to approve rezoning of the subject site to accommodate the proposal.

### *Official Community Plan Bylaw*

Most of the subject site is within the “Clyde Avenue area east of Taylor Way” development permit area (DPA). The proposed OCP amendment aims to expand the Clyde Avenue Area East of Taylor Way DPA to include 694 Duchess Avenue (**Appendix C**).

### *Zoning Bylaw*

The subject site, consisting of four legal lots, is “split zoned”. The northern portion of the site, 694 Duchess Avenue, is zoned RS3 (Single Family Dwelling Zone 3) and the remaining area to the south (671, 685, and 693 Clyde Avenue) is zoned CD30 (Park Royal North). The RS3 zone allows for single-family dwelling uses and the CD30 zone allows for at-grade parking and a parking ramp in that particular area of CD30. A rezoning is required to accommodate the proposal.

The proposed Comprehensive Development Zone 62 (CD62 - 671, 685, 693 Clyde Avenue and 694 Duchess Avenue) is a site-specific zone that reflects the development proposal. As an associated aspect of the

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<sup>1</sup> Land located within a radius of 800 metres from an intersection of a MOTI controlled access highway

rezoning, the parking rate applicable to Park Royal North will be modified (**Appendix D**).

### **3.0 Official Community Plan and Council Strategic Objective(s)**

#### *Official Community Plan (OCP) – Development Permit Area and Taylor Way Local Area Plan*

Most of the site is within the Clyde Avenue area east of Taylor Way DPA which allows for significant redevelopment of sites fronting along Clyde Avenue. The DPA aims to support a mix of uses and encourages property consolidation to create larger parcels of land. A 2.0 floor area ratio (FAR) is supported in this DPA for proposals that deliver community benefits such as rental housing.

The subject site is also located within the Taylor Way Corridor Local Area Plan (LAP) Boundary<sup>2</sup> (**Appendix A**). The OCP directs the creation of a local area plan for the Taylor Way Corridor to create additional housing capacity of approximately 500-600 new housing units. The public consultation and planning process for Taylor Way Corridor has not yet been initiated.

Prior to the adoption of a LAP, the development proposed can be reviewed in compliance with the Clyde Avenue area east of Taylor Way DPA guidelines, which allows for increased density for developments with a community benefit (such as rental housing), and in conjunction with OCP Policy 2.1.15 that allows for consideration of rezoning proposals within a LAP boundary by:

- a. Applying relevant District-wide policies contained in this plan and any existing area specific policies and guidelines; and
- b. Requiring the proposal's contribution to rental, non-market or supportive housing, or its advancement of low-carbon construction, or its ability to forward the public interest or provide other community benefits as determined by Council.

The OCP also includes Policy 2.1.17 that supports securing new purpose-built market and non-market rental, seniors and supportive housing units in appropriate locations close to transit and amenities by:

- a. Incentivizing new rental units through bonus density, increased height, and available zoning tools;
- b. Considering cash-in-lieu contributions to the District's Affordable Housing Fund when preferable for meeting the District's housing objectives;
- c. Considering financial incentives for non-market rental units (e.g., the reduction of development fees or charges, tax incentives);

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<sup>2</sup> The final boundaries of the Taylor Way Local Area Plan will be considered and confirmed through the subsequent Taylor Way local area planning process.

- d. Reducing off-street parking requirements; and
- e. Securing market and non-market rental housing units in perpetuity through Housing Agreements and available zoning tools.

#### *2021 – 2022 Council Strategic Goals and Objectives*

Applicable strategic goals and objectives:

- Goal 1.0: Significantly expand the diversity and supply of housing, including housing that is more affordable.
  - Objective 1.1: Approve an average of 250 net new housing units each year, including accessible housing, of which approximately 100 are market or non-market rental, approximately 75 are infill or missing middle (such as coach houses, multiplexes, and townhouses), and approximately 75 are strata apartment or mixed-use.

## **4.0 Financial Implications**

### **4.1 Community Amenity Contribution**

New developments are to deliver community amenities related to the impacts of new development. The value of the amenity is proportional to the increased potential of land use in comparison with existing zoning and land uses onsite. District policy defines a range of appropriate amenities, including housing affordability and diversity, childcare and cultural facilities, heritage preservation, public art, and public space, parks and the environment.

Consistent with District policy and through a negotiated approach, the applicant offers a voluntary community amenity contribution (CAC) of \$4,000,000 in cash. District staff consulted with a third-party financial consultant (at the cost of the developer) to determine the voluntary CAC. Staff recommend acceptance of the CAC that would flow into the District's amenity reserve account(s) to contribute to future amenity projects as determined by Council.

### **4.2 Infrastructure Upgrades**

Redevelopment of the site will require in-ground servicing infrastructure upgrades and a new sidewalk along Clyde Avenue. As the Taylor Way corridor is under the jurisdiction of the MOTI, the final boulevard design will be subsequently ratified as part of the development process at the expense of the developer. All required servicing upgrades and public realm improvements would be paid for by the applicant and secured through a development covenant.

The developer would also be required to provide Development Cost Charges at the building permit stage.

## 5.0 Background

### 5.1 Previous Decisions

Council considered the preliminary development proposal on October 4, 2021 and resolved to allow the development to proceed with public consultation prior to the adoption of the Taylor Way Local Area Plan.

## Analysis

### 5.2 Discussion

#### Site Context

The site, with an approximate area of 4,056 m<sup>2</sup> (43,659 sq. ft.), is located on the east side of Taylor Way. The site is situated on the south edge of the Taylor Way LAP boundary just north of the Marine Drive LAP boundary (**Appendix A**). The “L-shaped” site is comprised of three lots fronting Clyde Avenue, as well as a single lot fronting Duchess Avenue. The current development permit area, along Clyde Avenue, supports redevelopment of the site provided that developments include a defined community benefit such as rental housing. The site is vacant and has not been developed with the southern portion currently serving as an ancillary parking lot for Park Royal North commercial uses. The north edge of the site is characterized by an embankment with mature vegetation (Figure 1).

Existing land uses adjacent to the site include are listed below and seen in Figure 1:

- **East:** a 5-storey seniors housing complex
- **South:** a 6-level parking structure (across Clyde Avenue);
- **West:** Park Royal North
- **North:** and Northeast: 2-storey single family residential housing



Figure 1 – Aerial view of Clyde Avenue area east of Taylor Way

The site includes a private easement on the southwest portion of the site to allow for Park Royal North’s exit ramp.

As indicated in Figure 1, Clyde Avenue provides vehicular access to several multi-family developments and senior housing complexes east of Taylor Way. The 89-unit “Executive Parc” development, approved on December 15, 2021, at the corner of Taylor Way and Marine Drive will be provided vehicular access from the 6th Avenue cul-de-sac which is accessed via Clyde Avenue.

### **Proposal**

The applicant<sup>3</sup> proposes to rezone the site to allow for a 201 unit market rental apartment building proposal within a six-storey format (Figure 2). The architectural and landscape package form part of **Appendix E** which includes numerous images, perspectives, and extensive design rationale.



Figure 2 – Rendering of proposal (looking northward from Clyde Avenue)

The proposal aims to deliver needed rental housing for the community. The architecture is consistent with applicable development permit area guidelines and with the emerging built form in the Park Royal area. The proposal includes smaller “micro-unit” rental apartments which will be more affordable for potential tenants.

The development proposal will require rezoning the site to a site-specific Comprehensive Development Zone 62 (CD62 - **Appendix D**). The CD62 introduces “rental only” zoning by limiting residential tenure to rental only which negates the need for a housing agreement. Upon completion, the applicant plans to own and operate the rental building.

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<sup>3</sup> Larco Investments Ltd. which owns Park Royal

The proposed development includes:

- 201 residential apartment units (100% market rental units);
  - 79% “Micro-units” (350 sq. ft.);
  - 8% Studio Units (362 to 378 sq. ft.); and
  - 13% Adaptable Units (466 to 497 sq. ft.).
- An approximate FAR of 2.04<sup>4</sup>;
- 8,271.24 m<sup>2</sup> (89,031 sq. ft.) of floor area;
- Approximate 36% site coverage;
- 21.6 m (71 ft.) maximum height;
- 6 storeys maximum with rooftop amenity area;
- 1 level of underground parking (accessed from the southeast corner of the development including 50 parking stalls in total (40 for residents and 10 for visitors<sup>5</sup>);
- 283 secure bicycle storage stalls (in the underground parking area);
- 20 transitory bicycle storage stalls (at grade near the front entrance);
- Expansive rooftop deck, front plaza, and ground level outdoor areas;
- Large community outdoor area with small trail network with naturalized landscaping on the northwest portion of the site; and
- 2,700 sq. ft. of indoor amenity areas (including exercise gym, social space and lounge areas on the upper floors).

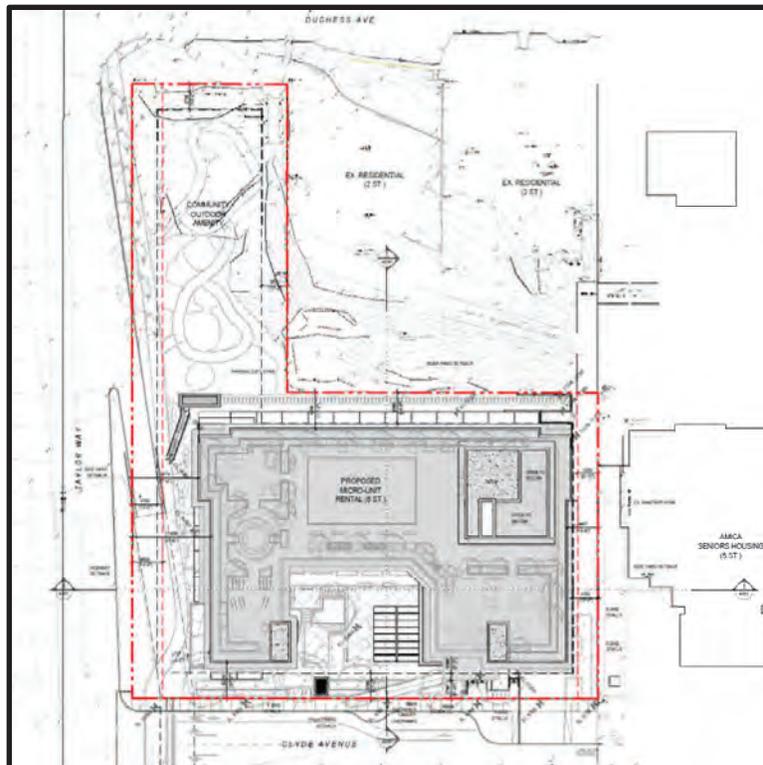


Figure 3 – General Site Plan

<sup>4</sup> The 2.04 FAR is spread over the entire site

<sup>5</sup> Each residential parking stall and at least 4 visitor stalls will include a 240-volt Electrical Vehicle (EV) charging capacity

The general siting, proposed boulevard improvements and on-site landscaping can be seen in Figure 3. The “Community Outdoor Amenity” (seen on the northwest portion of the site) is slated to be accessible to the public through registration of a statutory right-of-way (SROW) which will be secured through the development process. The ongoing maintenance of the area would be the responsibility of the building owner/operator<sup>6</sup>.

*Unit Size and Configuration*

The typical configuration of a “micro-unit” can be seen in Figure 4 and can be summarized as follows:

- The smaller units will provide relative affordability for residents;
- Flexible space provides for all the necessities of living, dining, sleeping, relaxing and entertaining;
- In-suite laundry is provided with a combination washer & dryer for each unit;
- Efficient interior design in the kitchens and bathrooms;
- Energy efficient appliances coupled with a high-performance building to reduce energy consumption; and
- The shared amenity areas are designed to complement the efficient suite layouts with over 2,000 sq. ft. of indoor amenity area, as well as the expansive rooftop outdoor amenity garden.

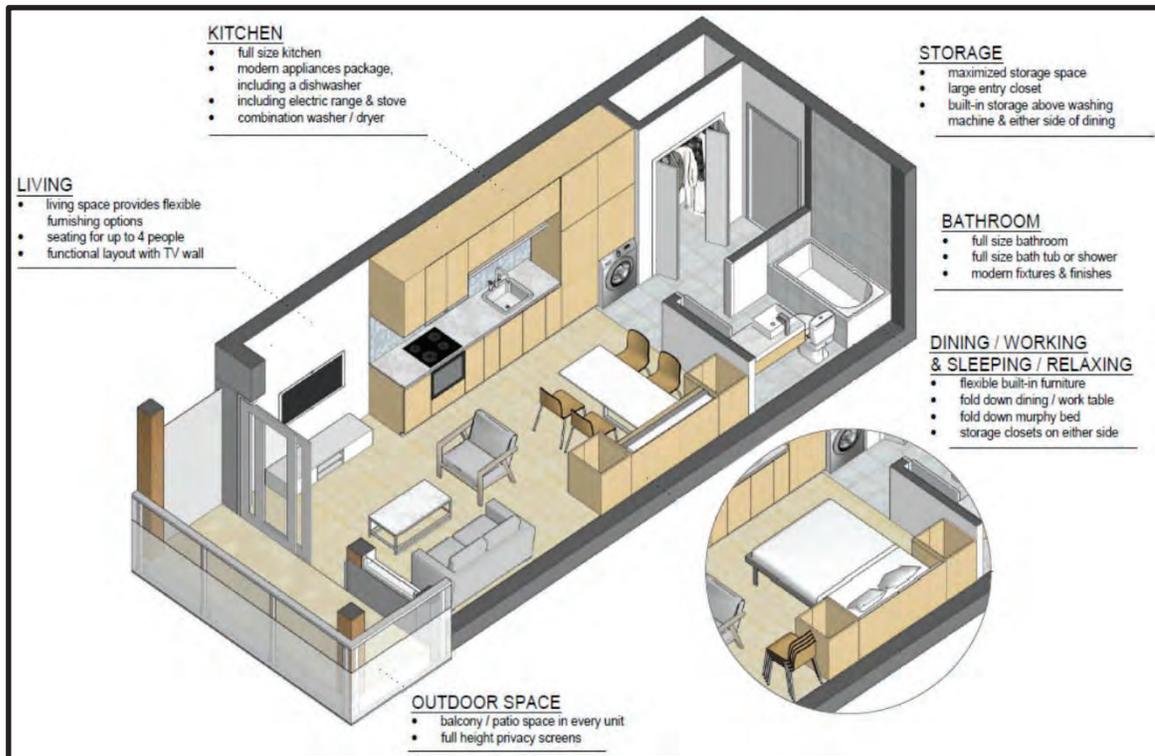


Figure 4 – Rendering of typical micro-unit

<sup>6</sup> As the proposed CD62 zoning only allows for rental residential tenure, the building may not be stratified and is to remain under one owner.

### *Accessibility*

The development proposal was reviewed and supported by the North Shore Advisory Committee on Disability Issues (ACDI) on March 24, 2022. A total of 27 units (which represents 13% of the units) in this development are designed to meet the Level 2 Adaptability Standard and those units are slightly larger than the typical “micro-unit” to allow for more manoeuvrability<sup>7</sup>. The typical layout of an accessible unit with enhancement features is seen in **Appendix F**.

### *Design Review Committee*

The development proposal was reviewed by the Design Review Committee (DRC) on February 10, 2022. At that meeting, the DRC resolved to require resubmission of the proposal to address several design concerns. The proposal was subsequently considered by the DRC on April 12, 2022. The DRC supported the revised proposal subject to further design development with staff on specified items. The applicant provided further information and improvements to the development to address DRC comments and staff are satisfied with the responses. Final specific design improvements included:

- Provision of an expansive covered outdoor kitchen area on the roof top deck and ground-level covered gazebo which offers protection from the elements.
- Extending the overhang at the front entrance to provide more protection from the elements and pronouncement of the entrance area.
- Improvements incorporated under the northbound on-ramp to contribute to the public realm.
- A 1' high and 6" deep metal break-shape profile was added on the front elevation to create a strong horizontal transition line between the brick and the cementitious panel above.
- The DRC recommended an increased amount of glass in the two primary stair towers facing Clyde Avenue. Subsequently, the amount of glass remained unchanged as increasing the amount of glazing would create a hotter, south facing space requiring extraneous mechanical ventilation. Without ‘overpowering’ the design, the current glazed stairwells offer a balanced design with attractive lantern-like elements that will provide an ambient glow at night, with ample daylight and views out during the day (Figures 5 and 6).

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<sup>7</sup> Adaptable units will range from 466 sq. ft. to 497 sq. ft.



Figure 5 – Rendering of proposal (indicating landscape improvements, elevator glazing, entrance improvements and metal band between brick and cementitious paneling)



Figure 6 – Rendering of building (with rooftop deck shown prominently)

### *OCP Amendment*

The southern portion of the site (671, 685, and 693 Clyde Avenue) is located within the OCP’s “Clyde Avenue Area East of Taylor Way” development permit area. An OCP amendment (**Appendix C**) is proposed to expand the Clyde Avenue East of Taylor Way Development Permit Area to include 694 Duchess Avenue (Figure 7). The same development permit area form and character guidelines will be applicable, yet with a slightly amended boundary.

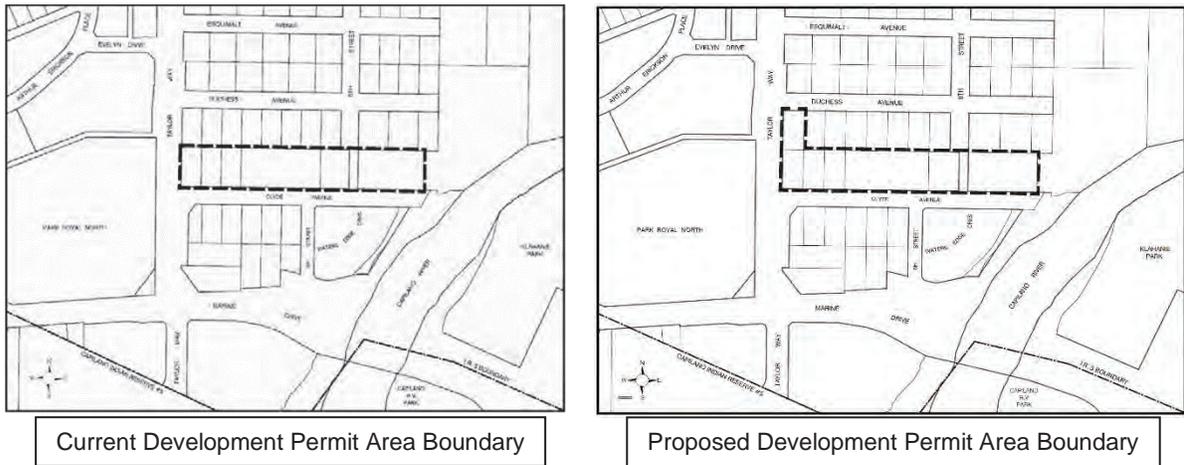


Figure 7 – Proposed mapping change (OCP amendment)

**Development Permit**

Staff have reviewed the proposal against the “Clyde Avenue Area East of Taylor Way Development Permit Area” form and character guidelines and support the development as outlined within proposed Development Permit 21-185 (**Appendix E**). The following provides an overview of primary policy directions within the guidelines and how the directions are met within the proposal:

- **Policy BF-D 2** states, “Allow for rezoning in the Clyde Avenue area east of Taylor Way to allow for a mix of uses including residential, mixed commercial residential buildings with street oriented commercial uses at grade, commercial office buildings, hotels and specialized residential facilities.”

The development includes rental housing with smaller/affordable “micro-units”. It should be noted that a commercial retail analysis was completed by the applicant which deemed that this site is not suitable for ground level commercial activities.

- **Policy BF-D 3** states, “Provide incentives for development to encourage property consolidation to create larger parcels of land with better opportunities for design and for improvements to the public street areas, and to encourage uses that would have defined community benefit.”
  - Allow for a density of development with a Floor Area Ratio (FAR) of up to 1.75 on properties of 20,000 sq. ft. or greater, and 1.0 on smaller properties.
  - A density bonus of up to 0.25 FAR may be considered for proposals that provide seniors’ care services, rental accommodations, public parkland, community use facilities (for example, child or adult daycare) or assisted and congregate care housing, subject to the registration of specific restrictions on title that will ensure retention of such use over the long term.”

The development of this larger site includes smaller/affordable “micro-unit” rental accommodations and public parkland<sup>8</sup> which are both community benefits and the development is within the FAR parameters for larger sites<sup>9</sup>.

- **Policy BF-D 4** states, “Encourage the highest quality of design in redevelopment of the Clyde Avenue area east of Taylor Way which reflects the mix of uses, the need for a landscaped setting adjacent to the Capilano River and to address the proximity of regional through traffic and major shopping area.”

The development includes significant landscaping throughout with a high-quality design and includes a coordinated boulevard design along Clyde Avenue that will complete and complement existing development to the east. The development also includes limited parking which reduces car dependency near a major shopping area and recurrent regional traffic congestion.

### **Transportation and Parking**

The applicant provided a Traffic Impact Assessment (TIA). As outlined in the next two sections, the assessment report provides a review of the traffic access and parking supply for the proposed 201 unit “market” rental development. In summary, staff have reviewed the TIA and are satisfied with the findings and support the proposed parking ratio and transportation improvements that would be delivered with the development.

#### *Projected Traffic Generation*

The proposed development, as outlined in the TIA, is anticipated to generate approximately 52 vehicle trips (combined inbound and outbound) during the weekday morning peak hour, 48 vehicle trips during the weekday afternoon peak hour, and 42 trips during the Saturday afternoon peak hour traffic period (or, on average, less than one vehicle per minute added to the area road system). The TIA concludes that The Taylor Way at Keith Road and the Taylor Way at Clyde Avenue intersections presently operate within capacity and the added trips by the proposed development are estimated to account for 0.6% of the total traffic at Taylor Way and Marine Drive during the weekday afternoon peak traffic period. Staff have reviewed and accept the findings of the TIA.

#### *Development Site Parking*

The development proposal includes 40 parking spaces available for residents and 10 visitor stalls within one level of underground parking. The proposed parking equates to 0.25 parking stalls per unit (including visitor stalls) which entails less than 1 stall per 4 units. The proposed parking supply rate is lower than the minimum supply requirement currently within

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<sup>8</sup> The public will be permitted to use the outdoor amenity area through registration of a SROW.

<sup>9</sup> The extra 0.04 FAR proposed (above the 2.0 FAR permitted within the guidelines) is permitted through “high performance buildings” regulations within the zoning bylaw.

the West Vancouver Zoning Bylaw for residential apartment land use which is intentional as a measure to attract future residents who are less reliant on private vehicle ownership than with conventional residential housing.

The TIA estimates car ownership will be considerably lower than typical apartment buildings within West Vancouver. The projected lower vehicle ownership and use was determined by reviewing the proposed housing type (rental), size of units, accessible location, available transit, bike storage and relies on data from other lower mainland town centres. Accordingly, the TIA determined the optimal number of resident and visitor stalls for the development.

The applicant will provide a 'post occupancy parking analysis report' to allow the District to better understand the development's use of the underground parkade and measure potential impact on street parking.

#### *Road Dedication and Northbound Overhead Onramp*

The final road specification along the Taylor Way corridor will require approval of the MOTI. As a condition of the rezoning, the applicant will dedicate 4.5 m (9.17 ft.) of road to the District of West Vancouver along Taylor Way to ensure sufficient space for potential vehicle lane realignment, bike, and pedestrian improvements.

Furthermore, as the northbound overhead onramp servicing Park Royal North onto Taylor Way may not be needed in the future<sup>10</sup>, the applicant has agreed to modify the terms of the existing easement agreement affecting the site to allow the 'onramp lands' to be transferred to the District of West Vancouver if the onramp is no longer needed in the future. This would allow future flexibility for potential road improvements along Taylor Way.

The planned improvements for Clyde Avenue include dedicated westbound turning lanes (allowing for dedicated lanes to turn either north or south onto Taylor Way), a bike lane on the north side of Clyde Avenue and a vehicle pullout for drop-offs in front of the primary entrance of the apartment building.

#### *Parking Rate Modification for Park Royal North (CD30)*

The southern portion of the development site is currently within "Area 3" of the CD30 zone which, in part, regulates land use and parking for Park Royal North (Figure 8). The proposed zoning amendment would remove the development site from the CD30 which will affect the required parking rate for Park Royal North.

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<sup>10</sup> Depending on future vehicle circulation requirements for Park Royal

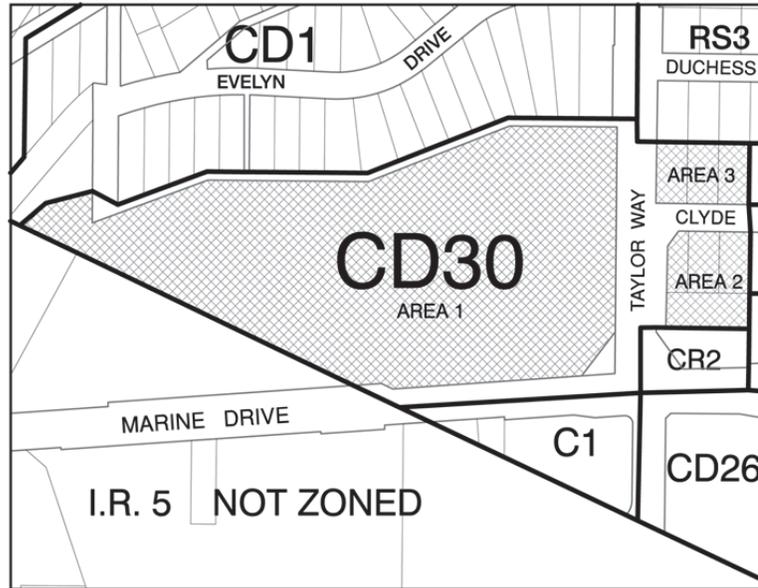


Figure 8 – Excerpt of CD30

Actual available for Park Royal North is indicated in Figure 9 on the next page. Upon detailed review of the CD30, the parking stalls located within I.R. No. 5. cannot be used within CD30 parking calculation (as those lands are not within the CD30 or within the jurisdiction of the District). Accordingly, to update the parking regulations for Park Royal North, the proposed zoning amendment is slated to update the CD30 parking requirement rate from 5 parking spaces per 92.9 m<sup>2</sup> of gross floor area (excluding pedestrian mall areas) to 3.6 parking spaces per 92.9 m<sup>2</sup> of gross floor area.

The revised parking rate works out to a minimum of 1 parking space for every 25.8 m<sup>2</sup> of gross floor area. As a direct comparison, the Ambleside commercial areas currently require a minimum of 1 parking space for every 37 m<sup>2</sup> of gross floor area.

In effect, Park Royal North still has 1,749 available parking stalls<sup>11</sup> and the development proposal is not taking away any useable parking from Park Royal North given that the current development site has never been used for day-to-day parking for Park Royal North. Furthermore, the modified parking rate ensures the correct parking ratio is included with the zoning for the site which effectively limits the allowable floor area of Park Royal North. Any subsequent increase in proposed floor area for Park Royal North would necessitate a zoning amendment and further consideration by Council.

<sup>11</sup> Actual number of stalls available to Park Royal North Mall (in Area 1 & 2 within CD30 and I.R.5)  
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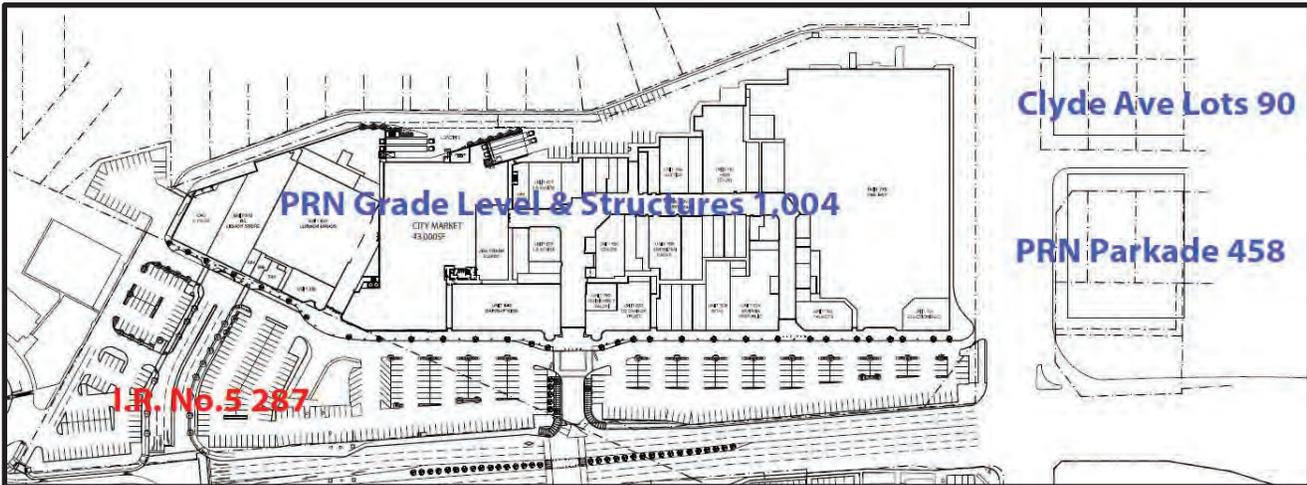


Figure 9 – Actual Available Parking Map for Park Royal North

**Sustainability**

New development proposals within the District of West Vancouver deliver high-performance buildings, in compliance with the BC Energy Step Code required by the Building Bylaw. All rezoning applications are expected to be compliant with “Sustainable Buildings Policy” that states “*New buildings should comply with the Low-carbon Energy System pathway and should meet the step higher of the BC Energy Step code than is otherwise required by Building Bylaw No. 4400, 2004.*” Accordingly, the development will be required to attain Step 3 and utilize a low-carbon energy system.

All residential parking spaces, and at least 4 visitor stalls, will be required to comply with the zoning bylaw to include EV charging and secure bicycle storage is proposed within the parkade.

The proposal, with smaller and therefore more affordable units, will be available to people who work within West Vancouver contributing to sustainability goals by allowing employees to live and work within West Vancouver. In addition, the smaller housing type may also aid with older tenants wishing to downsize.

**5.3 Public Engagement and Outreach**

*Public Information Meetings*

Prior to submission of the formal application, and in compliance with District public engagement requirements, the applicant completed an in-person Preliminary Public Information Meeting on November 24, 2021, prior to formal submission. A summary of the meeting is provided in **Appendix G**.

Should the proposal advance, the applicant will be required to advertise and conduct a second information meeting prior to the public hearing.

*Signage*

Should the proposal advance, the applicant will be required to install a

development information sign with particulars about the second public information meeting and public hearing.

#### *Public Hearing and Notification*

A public hearing must be held to consider the proposed bylaws. The recommendation herein projects the public hearing to be held on December 12, 2022. Notice of the public hearing and consideration of the development permit will be given in accordance with District procedures.

#### *Website*

In alignment with current practise, a description of the proposal, applicable dates and architectural drawings have been placed on the District website. The website will be updated should the proposal advance.

#### 5.4 Conditions Precedent to Adoption

Prior to adoption of the bylaws and approval of the development permit, the following requirements must be met:

- Payment of the community amenity contribution;
- Registration of a requisite development covenant<sup>12</sup>; and
- Approval of Zoning Bylaw No. 4662, 2010, Amendment Bylaw No. 5223, 2022 by MOTI.

### **6.0 Options**

#### 6.1 Recommended Option

Council give first reading to the proposed bylaws and set a date for a public hearing and a concurrent public meeting.

#### 6.2 Considered Options

- a) give first reading to the proposed bylaws and set an alternative date (to be specified) for a public hearing and concurrent public meeting; or
- b) defer consideration of the proposal pending the receipt of additional information (to be specified) to assist in the consideration of the application; or
- c) reject the application.

### **7.0 Conclusion**

Staff assessment of this application has concluded that the proposal is appropriate and supportable based on relevant OCP policy. Specifically, the proposal is generally consistent with the Clyde Avenue Area East of Taylor Way Development Permit Area and meets the intent of OCP policy 2.1.15, which allows for consideration of this market rental proposal prior

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<sup>12</sup> The development covenant will ensure dedication of roadway (along Taylor Way), secure public realm improvements and provision of a development servicing agreement that would secure payment/construction of all required upgrades that would be determined through technical review at the building permit stage.

to adoption of the Taylor Way Local Area Plan. The OCP also includes Policy 2.1.17 that supports securing new purpose-built rental market housing units (among other housing types) in appropriate locations close to transit and amenities.

As well, as a primary aspect of sustainability, good land use planning principles include allowing for higher density development in locations that have convenient access to established public transit, amenities, services, and recreational opportunities. Accordingly, the site's central location will contribute to reduced car dependency, encourage transit use, walking and cycling to nearby shops and services. Overall, the proposed rezoning will facilitate development recognized as a more sustainable and efficient land use within the OCP and not unduly affect traffic and parking in the area. Staff recommends that the proposed bylaws be given first reading and a date for a public hearing and concurrent public meeting for DP21-185 be scheduled.

Author:   
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Erik Wilhelm, Senior Community Planner

Concur:   
\_\_\_\_\_  
Michelle McGuire, Senior Manager of Current Planning and Urban Design

Date: November 2, 2022 Page 18  
From: Erik Wilhelm, Senior Community Planner  
Subject: Proposed Zoning Amendment, OCP Amendment and Development Permit for 671,  
685, 693 Clyde Avenue and 694 Duchess Avenue

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Appendices:

A - Context Map (Showing Local Area Plan Boundaries)

B - Project Profile

C - Official Community Plan Bylaw No. 4985, 2018, Amendment Bylaw No. 5222,  
2022

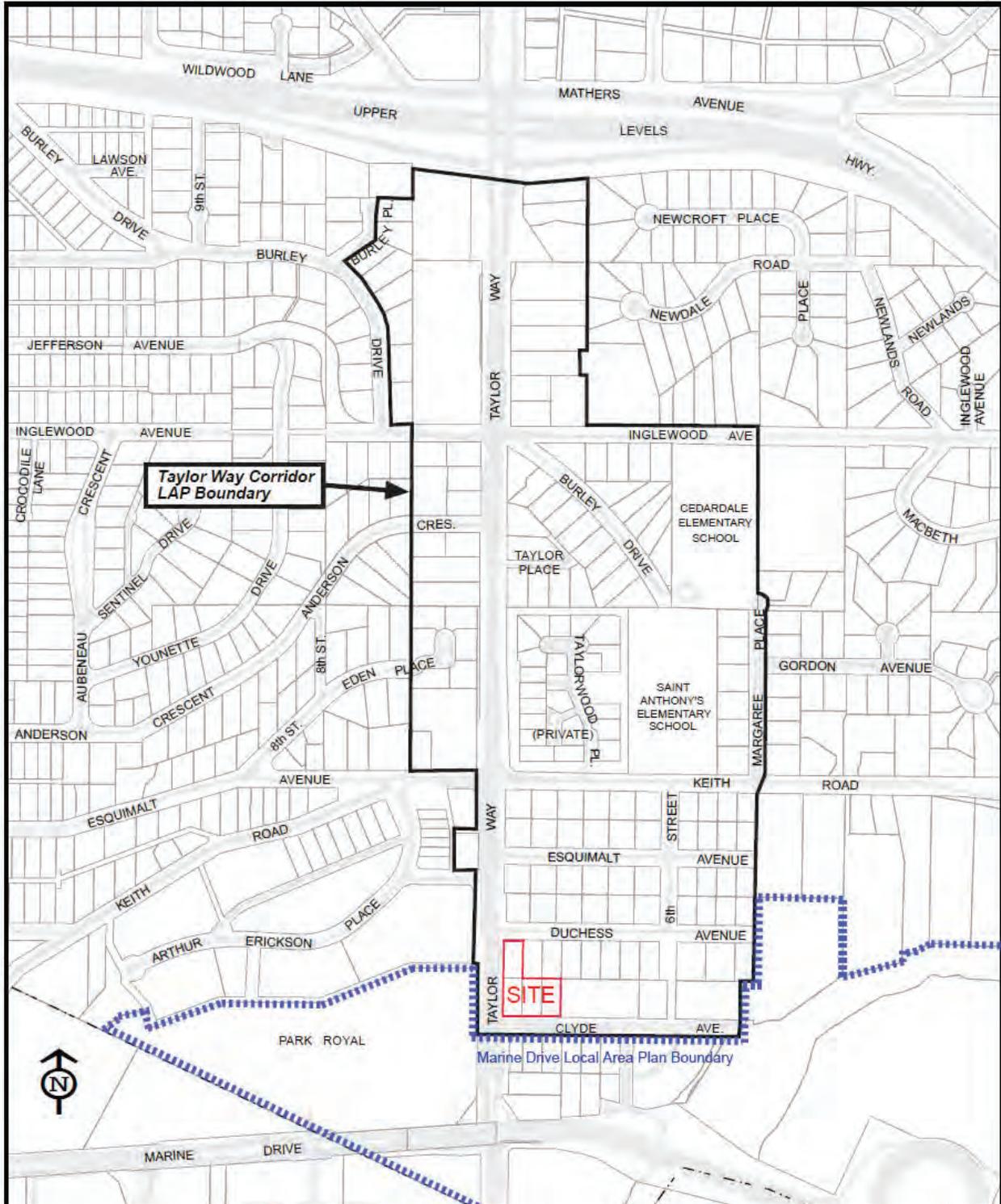
D - Zoning Bylaw No. 4662, 2010, Amendment Bylaw No. 5223, 2022

E - Proposed Development Permit 21-185

F - Typical Layout of Accessible Units Rendering

G - Public Engagement Summary

# APPENDIX A – CONTEXT MAP



## CONTEXT PLAN

Taylor Way Local Area Planning Boundary Map

DATE:	September 2022	REV:	0
FILE:	<b>SK-1</b>		

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## APPENDIX B – PROJECT PROFILE

<b>Project:</b>	<b>Park Royal North - “Micro-suites”</b>
<b>Application:</b>	File No. 21-185
<b>Applicant:</b>	Park Royal (Larco Investments Ltd.)
<b>Architect:</b>	DA Architects + Planners
<b>Landscape Architect:</b>	Vaughan Landscape Planning & Design
<b>Property Address:</b>	694 Duchess Avenue
<b>Legal Description:</b>	LOT 25 DISTRICT LOT 1039 PLAN 2127
<b>PID:</b>	014-020-394
<b>Existing Zoning:</b>	RS3
<b>Site Area:</b>	1,014 m <sup>2</sup>
<b>Property Address:</b>	671 Clyde Avenue
<b>Legal Description:</b>	LOT 38 DISTRICT LOT 1039 PLAN 2127
<b>PID:</b>	014-020-581
<b>Existing Zoning:</b>	CD30
<b>Site Area:</b>	1,014 m <sup>2</sup>
<b>Property Address:</b>	685 Clyde Avenue
<b>Legal Description:</b>	LOT 39 DISTRICT LOT 1039 PLAN 2127
<b>PID:</b>	014-020-611
<b>Existing Zoning:</b>	CD30
<b>Site Area:</b>	1,014 m <sup>2</sup>
<b>Property Address:</b>	693 Clyde Avenue
<b>Legal Description:</b>	LOT 40 DISTRICT LOT 1039 PLAN 2127
<b>PID:</b>	014-020-637
<b>Existing Zoning:</b>	CD30
<b>Site Area:</b>	1,014 m <sup>2</sup>
<b>Primary OCP Policies:</b>	2.1.15 and 2.1.17
<b>Guidelines:</b>	Clyde Avenue East of Taylor Way
<b>Zoning (Proposed):</b>	CD62 (671, 685, 693 Clyde Avenue and 694 Duchess Avenue)
<b>Proposal:</b>	To consolidate 4 lots and rezone the site to CD62 for a 6-storey market rental building with 201 units and public realm improvements.

Particulars	Proposed	Notes
FAR	2.04	Additional .04 permitted through "High Performance Buildings Section of the Zoning Bylaw
Site Coverage	36%	
Building Height	21.6m	
Number of Storeys	6	With rooftop amenity area
Building Floor Area (Gross)	8,897.45	
<b>Number of Units</b>		
Market Apartment	0	
Rental	201	
Total:	201	
<b>Setbacks</b>		
Front	3.7m	Before required dedications South lot line
Rear	4.5m	North lot line
Side	3.7m	East lot line
Side	9.6m	West lot line
<b>Parking</b>		
Residential:		
Regular Size	31	
Small Car	5	
Accessible	4	
Sub-Total:	40	0.2/unit (ratio)
Visitor		
Regular Size	6	
Small Car	3	
Accessible	1	
Sub-Total:	10	
Total Parking:	50	0.05/unit (ratio)
Bicycle Parking/Storage:	303	
EV Charging	Level 2 Charging	4 visitor stalls with Level 2 charging



District of West Vancouver

## **Official Community Plan Bylaw No. 4985, 2018, Amendment Bylaw No. 5222, 2022**

Effective Date:

District of West Vancouver

**Official Community Plan Bylaw No. 4985,  
2018,  
Amendment Bylaw No. 5222, 2022**

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District of West Vancouver

# **Official Community Plan Bylaw No. 4985, 2018, Amendment Bylaw No. 5222, 2022**

A bylaw to amend the Official Community Plan to include 694 Duchess Avenue within Development Permit Area BF-DF “Clyde Avenue Area East of Taylor Way”.

Previous amendments: Amendment bylaws 5008, 5045, 5054, 5057, 5064, 5074, 5076, 5120, 5128, 5135, and 5172.

WHEREAS the Council of The Corporation of the District of West Vancouver deems it expedient to provide for an amendment to the Official Community Plan to allow for the appropriate development of lands at 671, 685, 693 Clyde Avenue and 694 Duchess Avenue and include 694 Duchess Avenue within the “Clyde Avenue Area East of Taylor Way” development permit area;

NOW THEREFORE, the Council of The Corporation of the District of West Vancouver enacts as follows:

## **Part 1 Citation**

- 1.1 This bylaw may be cited as Official Community Plan Bylaw No. 4985, 2018, Amendment Bylaw No. 5222, 2022.

## **Part 2 Severability**

- 2.1 If a portion of this bylaw is held invalid by a Court of competent jurisdiction, then the invalid portion must be severed and the remainder of this bylaw is deemed to have been adopted without the severed section, subsection, paragraph, subparagraph, clause or phrase.

## **Part 3 Amends Area-Specific Policy**

- 3.1 Schedule ii of Official Community Plan Bylaw No. 4985, 2018 is amended as follows:

- 3.1.1 By amending Policy BF-D4 “Clyde Avenue Area East of Taylor Way” as follows:
- 3.1.2 On page 78 of Schedule ii forming part of Official Community Plan Bylaw No. 4985, 2018 delete Schedule A and insert in its place Schedule B.

## **Schedules**

Schedule A – Current “Development Permit Area Designation Map BF-D4”

Schedule B – Proposed “Development Permit Area Designation Map BF-D4”

READ A FIRST TIME (MAJORITY VOTE IN THE AFFIRMATIVE) on

PUBLICATION OF NOTICE OF PUBLIC HEARING on

PUBLIC HEARING HELD on

READ A SECOND TIME (MAJORITY VOTE IN THE AFFIRMATIVE) on

READ A THIRD TIME (MAJORITY VOTE IN THE AFFIRMATIVE) on

ADOPTED by the Council (MAJORITY VOTE IN THE AFFIRMATIVE) on

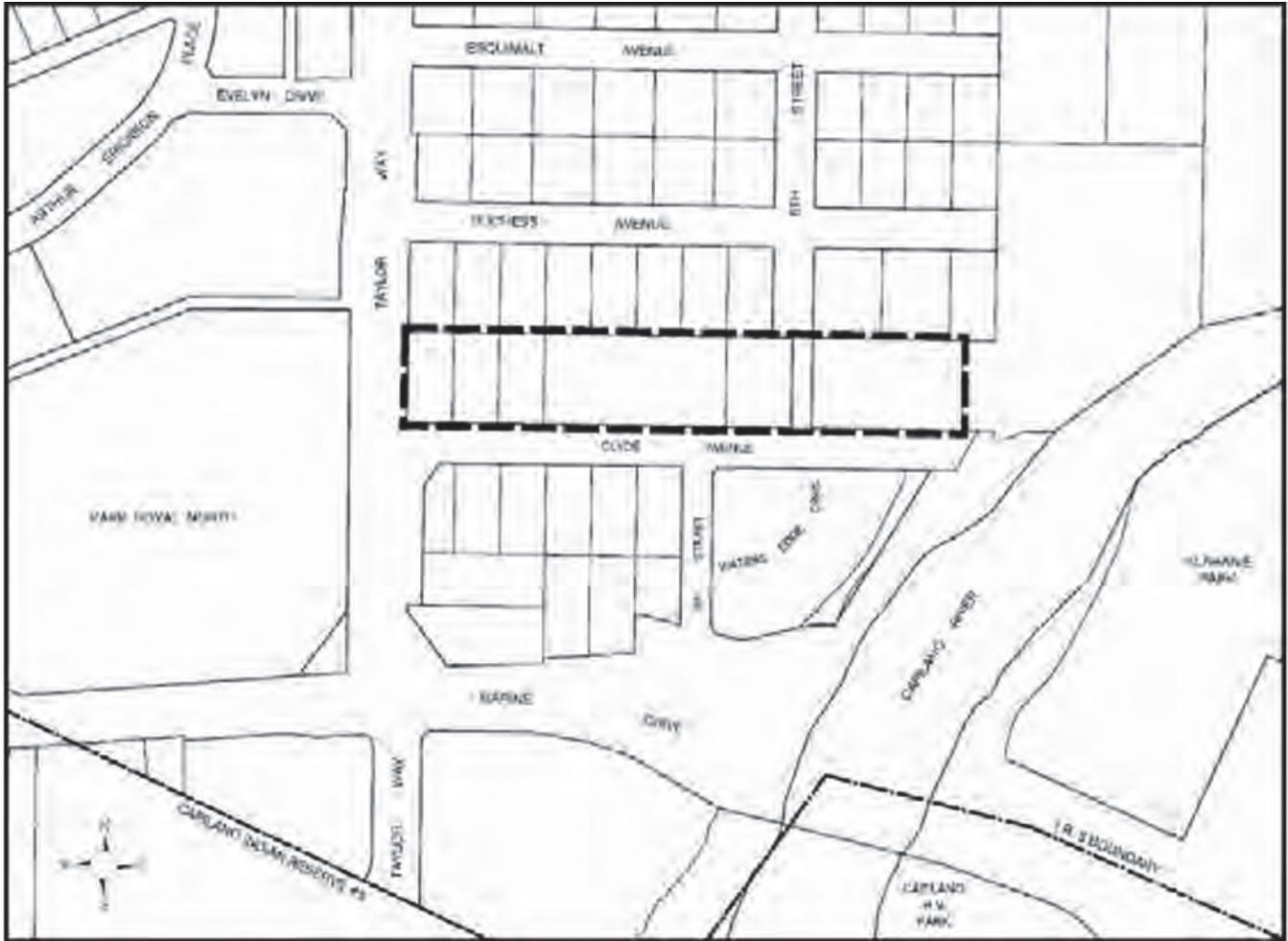
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Mayor

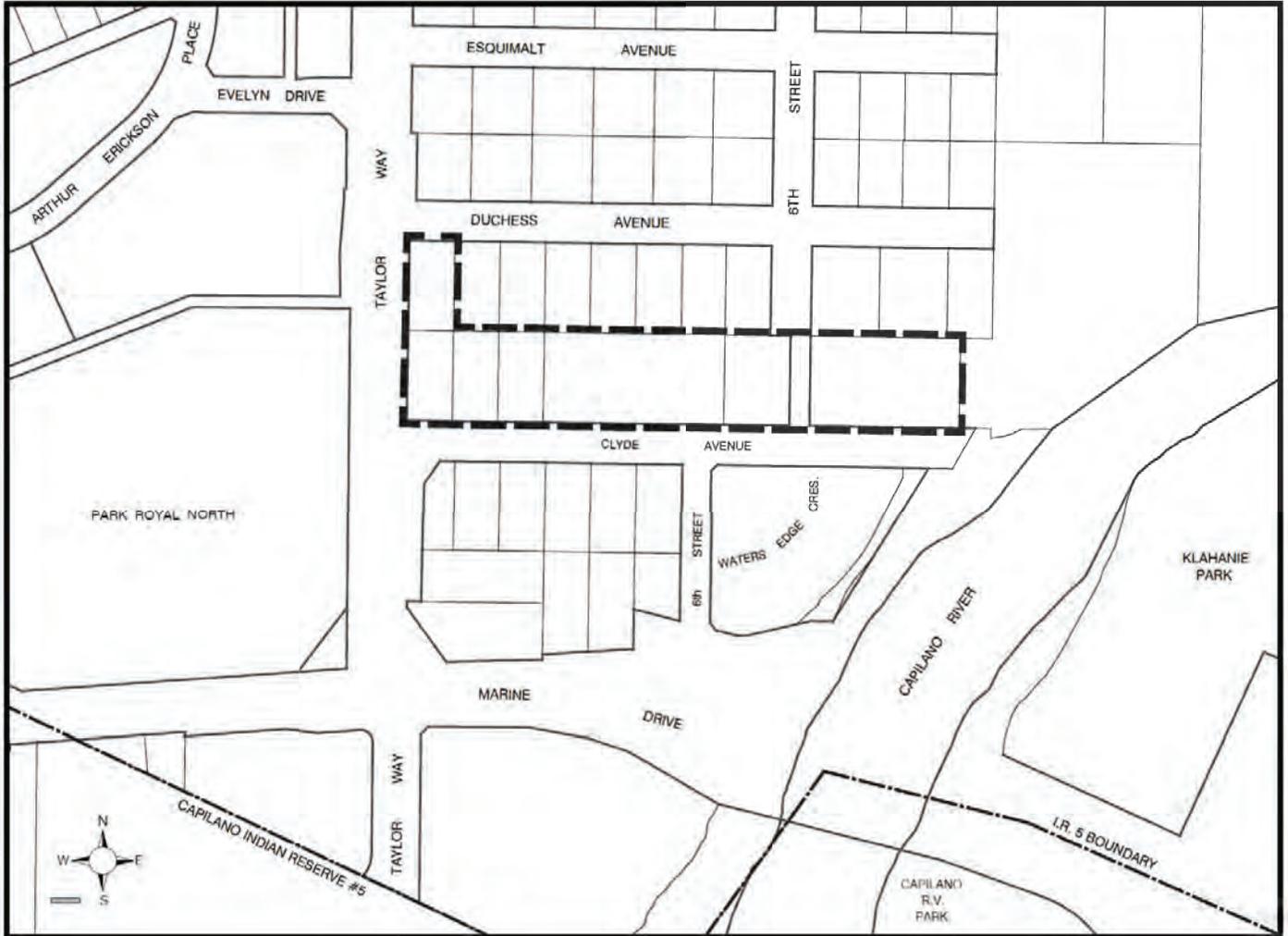
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Corporate Officer

### Schedule A – Current “Development Permit Area Designation Map BF-D4”



### Schedule B – Proposed “Development Permit Area Designation Map BF-D4”



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District of West Vancouver

**Zoning Bylaw No. 4662, 2010**  
**Amendment Bylaw No. 5223, 2022**  
(671, 685, 693 Clyde Avenue and 694 Duchess Avenue)

Effective Date:

# Zoning Bylaw No. 4662, 2010 Amendment Bylaw No. 5223, 2022

## Table of Contents

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Part 3	Adds CD62 Zone & Rezones the Lands .....	1
Part 4	Amends CD30 .....	2
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	Schedule D – Zoning Map Amendment.....	3

District of West Vancouver

## **Zoning Bylaw No. 4662, 2010 Amendment Bylaw No. 5223, 2022**

A bylaw to rezone 671, 685, 693 Clyde Avenue and 694 Duchess Avenue for a 201-unit rental apartment building development.

Previous amendments: Amendment bylaws 4672, 4677, 4678, 4679, 4689, 4701, 4680, 4710, 4697, 4716, 4712, 4737, 4726, 4736, 4757, 4752, 4767, 4787, 4788, 4784, 4772, 4791, 4805, 4809, 4828, 4854, 4873, 4866, 4895, 4839, 4898, 4927, 4944, 4905, 4974, 4967, 4982, 4962, 4928, 4992, 5001, 5021, 5024, 5028, 5009, 4938, 5044, 5055, 5051, 5068, 5065, 5087, 5069, 5110, 5106, 5132, 5161, 5160, 5013, 5122, 5155, 5169, 5192, 5175, 5171, and 5201.

WHEREAS the Council of The Corporation of the District of West Vancouver deems it expedient to provide for and amendment to the Zoning Bylaw;

NOW THEREFORE, the Council of The Corporation of the District of West Vancouver enacts as follows:

### **Part 1 Citation**

- 1.1 This bylaw may be cited as Zoning Bylaw No. 4662, 2010 Amendment Bylaw No. 5223, 2022.

### **Part 2 Severability**

- 2.1 If a portion of this bylaw is held invalid by a Court of competent jurisdiction, then the invalid portion must be severed and the remainder of this bylaw is deemed to have been adopted without the severed section, subsection, paragraph, subparagraph, clause or phrase.

### **Part 3 Adds CD62 Zone & Rezones the Lands**

- 3.1 Zoning Bylaw No. 4662, 2010, Schedule A, Section 600 (Comprehensive Development Zones) is hereby amended by adding Section 662 as the

- CD62 – Comprehensive Development Zone 62 (671, 685, 693 Clyde Avenue and 694 Duchess Avenue), as set out in **Schedule A** of this bylaw.
- 3.2 The Lands, as set out in **Schedule B** of this bylaw are rezoned from Single Family Dwelling Zone 3 (RS3) and Comprehensive Development Zone 30 (Park Royal North) (CD30) to Comprehensive Development Zone 62 (671, 685, 693 Clyde Avenue and 694 Duchess Avenue) (CD62).

## Part 4 Amends CD30

- 4.1 Zoning Bylaw No. 4662, 2010, Schedule A, Section 630.01 “Map” is amended by:
- 4.1.1 Deleting the current map and replacing it with the map seen in **Schedule C**.
- 4.2 Zoning Bylaw No. 4662, 2010, Schedule A, Section 630.02 “Permitted Uses” is amended by:
- 4.2.1 Deleting regulation “**(3)**” in its entirety.
- 4.3 Zoning Bylaw No. 4662, 2010, Schedule A, Section 630.052 “Off-Street Parking” is amended by:
- 4.3.1 Deleting Section 630.052(1)(a) in its entirety and replacing it with “3.6 parking spaces per 92.9 square metres of gross floor area.”
- 4.3.2 Adding the following to Section 630.052 (1):
- “(c) For the purposes of this section, when calculating the required number of parking spaces, the required number of parking spaces may be rounded down.”

## Part 5 Amends the Table of Contents

- 5.1 Zoning Bylaw No. 4662, 2010, Schedule A, Section 100 Table of Contents is amended accordingly to insert Comprehensive Development Zone 62 (671, 685, 693 Clyde Avenue and 694 Duchess Avenue).

## Part 6 Amends Zoning Map

- 6.1 Zoning Bylaw No. 4662, 2010, Schedule A, Section 852, Schedule 2, Zoning Maps is hereby amended by changing the zoning on the Lands as shown shaded on the map in **Schedule D** of this bylaw,

FROM: RS3 (Single Family Dwelling Zone 3) and CD30 (Comprehensive Development Zone 30 (Park Royal North)

TO: CD62 – Comprehensive Development Zone 62 (671, 685, 693 Clyde Avenue and 694 Duchess Avenue).

## **Schedules**

Schedule A - CD62 – Comprehensive Development Zone 62 (671, 685, 693 Clyde Avenue and 694 Duchess Avenue)

Schedule B - Rezoning Map

Schedule C – Revised CD30 Map

Schedule D - Amendment to Zoning Bylaw No. 4662, 2010, Schedule A, Section 852, Schedule 2, Zoning Index Map 2

READ A FIRST TIME on

PUBLICATION OF NOTICE OF PUBLIC HEARING on

PUBLIC HEARING HELD on

READ A SECOND TIME on

READ A THIRD TIME on

APPROVED by the Ministry of Transportation and Infrastructure on

ADOPTED by the Council on

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Mayor

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Corporate Officer

## Schedule A – CD62 Zone

### 662 CD62 (671, 685, 693 Clyde Avenue and 694 Duchess Avenue)

#### 662.01 Permitted Uses

- (1) Apartment building
- (2) Accessory buildings, structures and uses
- (3) Child care
- (4) Community care
- (5) Home based business
- (6) Necessary interconnected elevated ramps accessory to the parking structures in Area 1 and 2 of CD30

#### 662.02 Conditions of Use

- (1) Residential tenure is limited to *Residential rental tenure* within CD62
- (2) Only one storey of underground parking is permitted within CD62

#### 662.03 Floor Area Ratio (FAR)

- (1) Total maximum of 2.0 FAR
- (2) For the purposes of calculating FAR, the site area is 4,056.05 square metres, being the area prior to any required dedications

#### 662.04 Density

A maximum of 201 dwelling units

#### 662.06 Setbacks

Minimum:

Front (south – Clyde Avenue):	3.7 metres; however, the primary entrance canopy may protrude a maximum of 2.7 metres into the front yard setback
Rear (north):	4.5 metres
Side (east):	3.7 metres
Side (west):	9.6 metres; however, excluding 1 parkade exit staircase

#### 662.07 Building Height

- (1) Apartment building height is limited to a maximum height of 21.6 metres

- (2) Notwithstanding Section 120.19(2)(a), the height measurement shall not include elevator shafts and rooftop mechanical equipment

#### **662.08 Number of Storeys**

- (1) Maximum 6 storeys (rooftop outdoor amenity area, and associated rooftop enclosure)

#### **662.09 Off-Street Parking**

- (1) All underground parking shall comply with Section 142
- (2) Notwithstanding Section 142.10(1), at least 4 visitor stalls shall include an energized outlet that is:
  - (a) capable of providing Level 2 charging for an electric vehicle; and
  - (b) labelled for the use of electric vehicle charging.
- (3) Notwithstanding the parking reductions permitted with Section 143.01(4), the following must be provided and located within an underground parkade:
  - a) A minimum of 40 parking spaces available for residents; and
  - b) A minimum of 10 visitor parking spaces

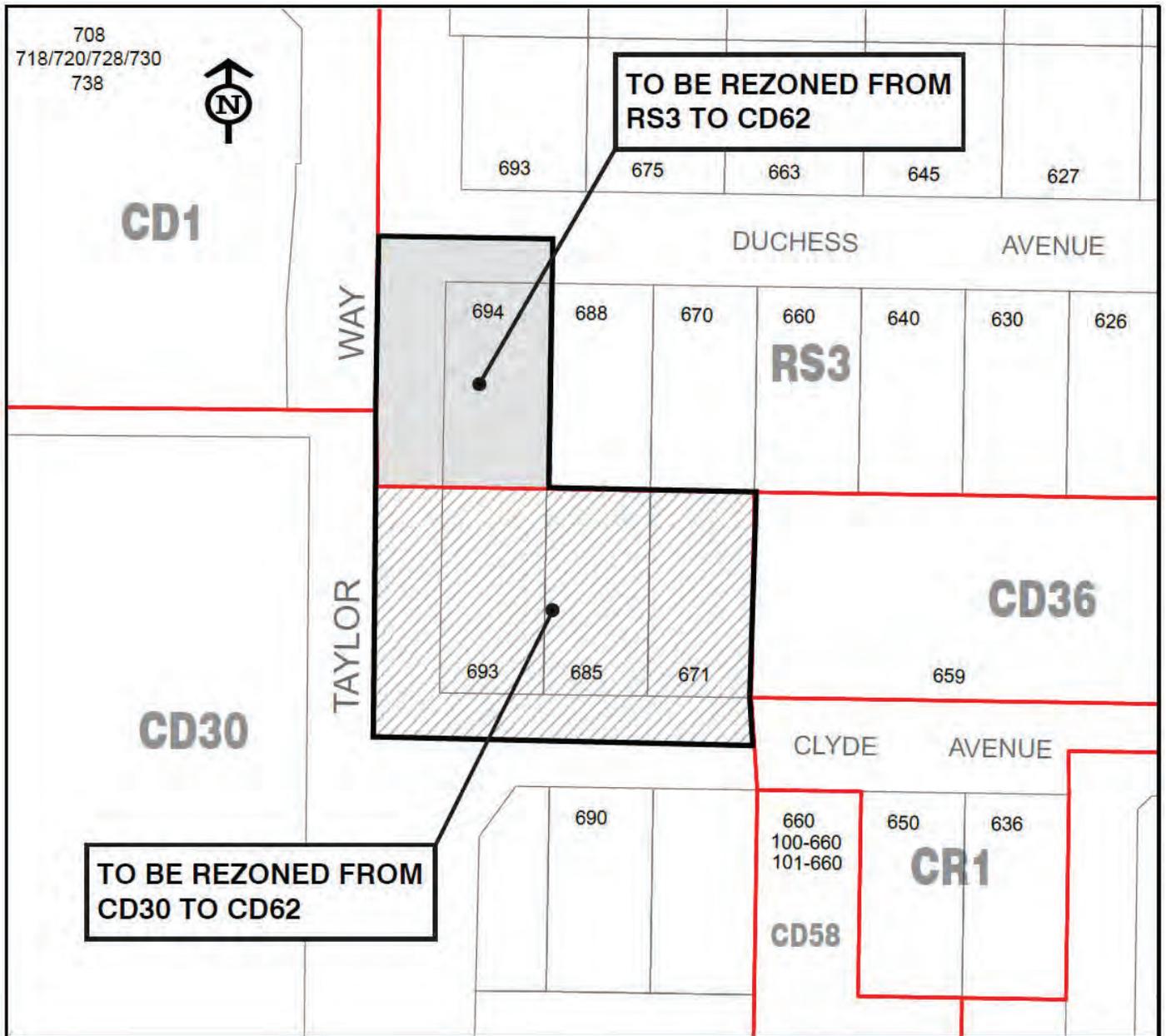
#### **662.10 Bicycle Parking**

- (1) The provision of secure bicycle parking shall comply with Section 143.01 of the Zoning Bylaw
- (2) Notwithstanding Sections 143.02(2) and 143.02(3), at least 0.1 short-term bicycle parking spaces per unit must be provided that all area permitted to be unsheltered

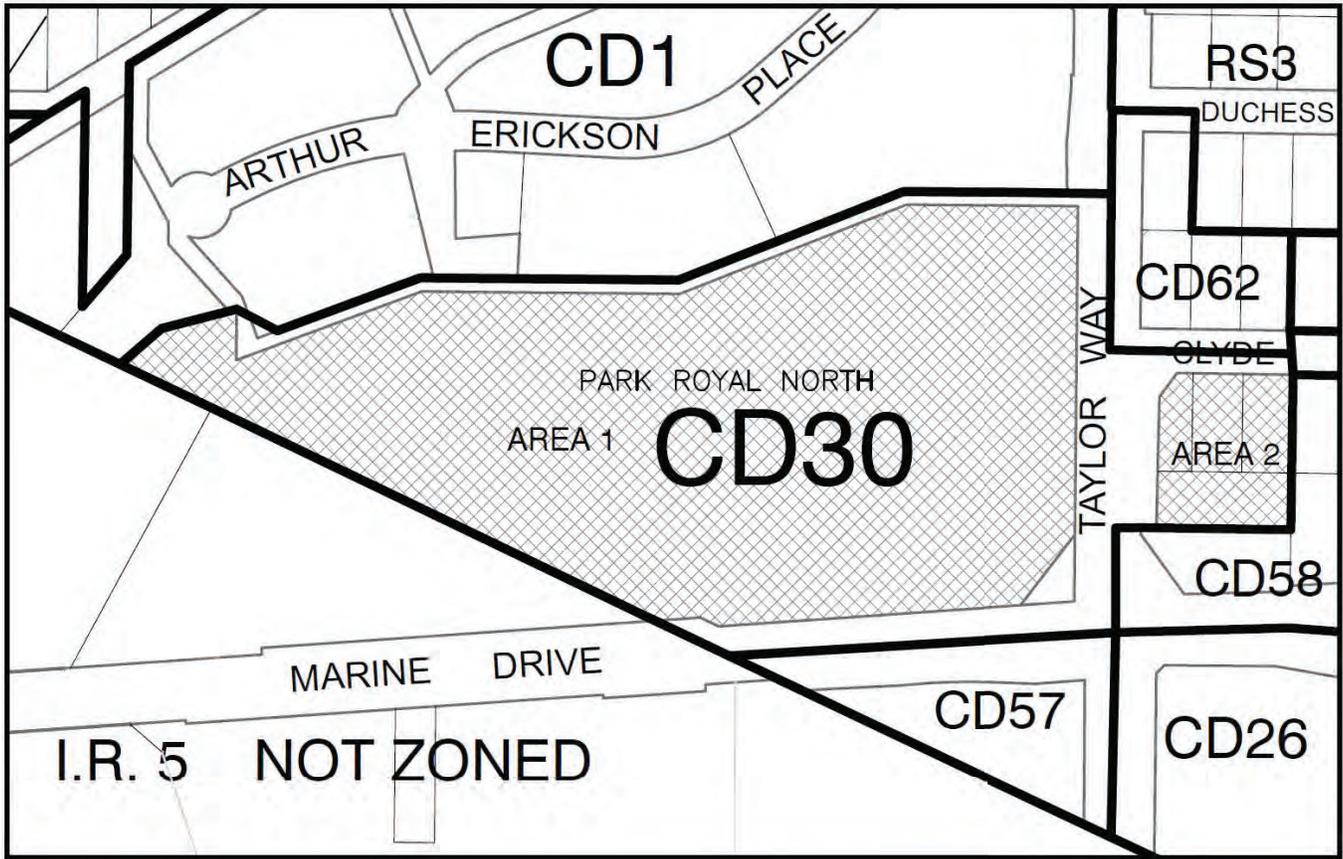
#### **662.11 Notes**

- (1) The minimum setbacks outlined in Section 662.06 are to be measured from the legal plans on record at the Land Titles and Survey Authority of British Columbia as of the effective date of CD62 prior to any required dedications.

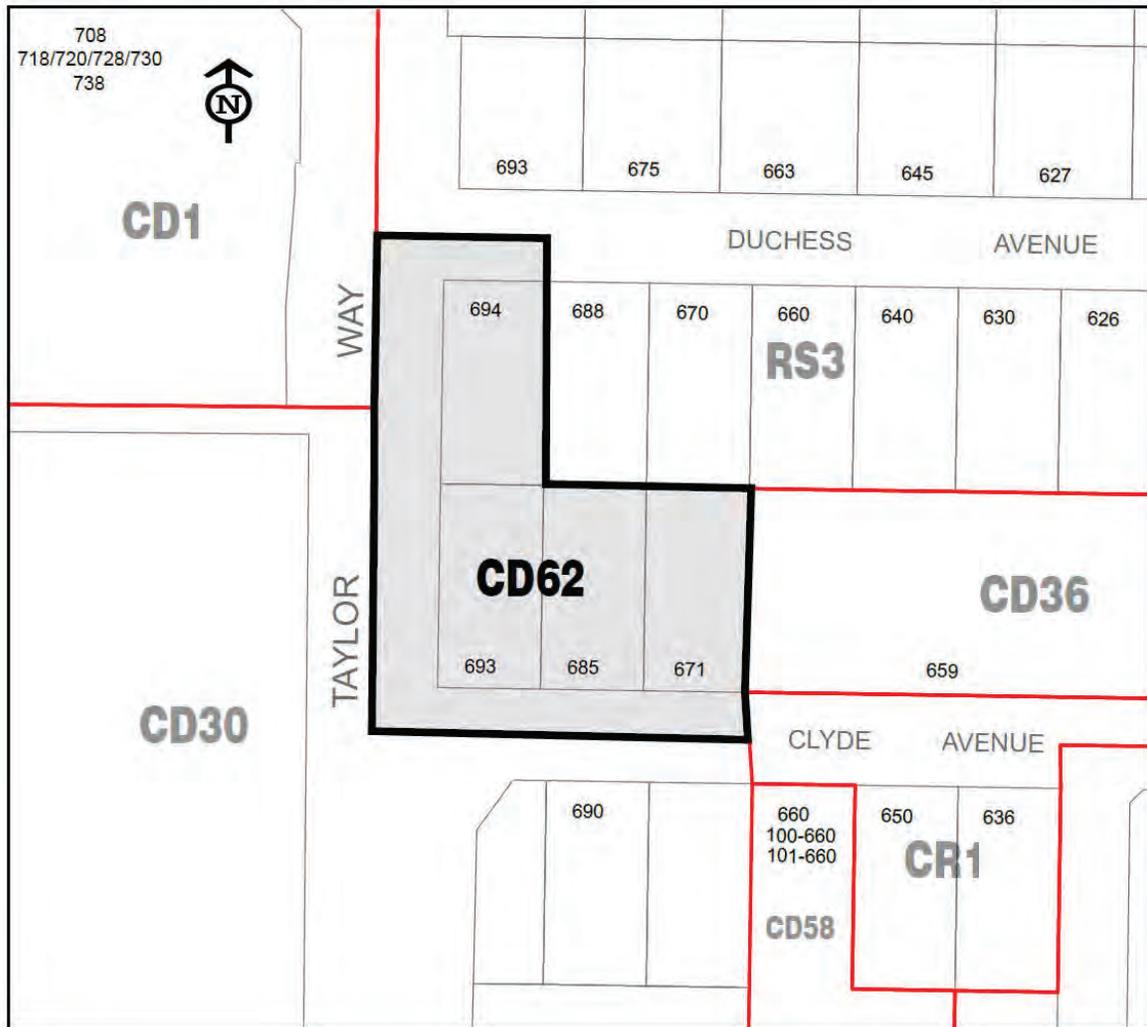
### Schedule B – Rezoning Map



### Schedule C – Revised CD30 Map



### Schedule D – Zoning Map Amendment



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## District of West Vancouver

### Proposed Development Permit No. 21-185

Current Owner: PARK ROYAL SHOPPING CENTRE HOLDINGS LTD., INC.NO.  
C-596935(3rd FLOOR – 100 PARK ROYAL, WEST VANCOUVER,  
BC, V7T 1A2)

This Development Permit applies to “the Lands”:

Civic Address: 671, 685, 693 CLYDE AVENUE AND 694 DUCHESS AVENUE

Legal Description: LOT 25 DISTRICT LOT 1039 PLAN 2127 (PID: 014-020-394)  
LOT 38 DISTRICT LOT 1039 PLAN 2127 (PID: 014-020-581)  
LOT 39 DISTRICT LOT 1039 PLAN 2127 (PID: 014-020-611)  
LOT 40 DISTRICT LOT 1039 PLAN 2127 (PID: 014-020-637)

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1. This Development Permit:
  - (a) imposes requirements and conditions for the development of the Lands, which are designated by the Official Community Plan within the “Clyde Avenue East of Taylor Way” Development Permit Area to promote the siting and design of mixed-use, commercial and multi-family development that reflects quality building design, materials and landscaping subject to Guidelines BF-D4 specified in the Official Community Plan; and
  - (b) is issued subject to the Owner’s compliance with all of the Bylaws of the District applicable to the Lands, except as specifically varied or supplemented by this Permit.
2. The following requirements and conditions shall apply to the Lands:
  - 2.1 Buildings, structures, and site development shall take place in substantial compliance with the drawings from DA Architects + Planners dated June 10, 2022 attached as Schedule “A”.
  - 2.2 On-site landscaping shall take place in substantial compliance with the drawings from Vaughan Landscape Planning & Design and DA Architects + Planners dated June 10, 2022 attached as Schedule “B”.
  - 2.3 Contingent on input from the Ministry of Transportation and Infrastructure, off-site infrastructure and roadworks “ *site r s*” along the frontage of the Lands shall take place in substantial compliance with Schedule “A” and Schedule “B”.
  - 2.4 The minimum energy performance of the development proposal shall conform to a minimum Step 3, with a Low-Carbon Energy System, as defined by the British Columbia Energy Step Code. Compliance with

the required step shall be demonstrated through provision of sufficient documentation to the satisfaction of the Chief Building Official.

3. Prior to commencing site work or Building Permit issuance, whichever occurs first, the Owner must:
  - 3.1 Provide and implement a plan for traffic management during construction to the satisfaction of the District's Manager of Development Engineering.
  - 3.2 Apply for, and receive a tree permit to allow for proposed tree removal as outlined in the "Tree Removal Plan" as outlined in Schedule "B". This will require installation of tree, vegetation and/or hedge protection measures as required to the satisfaction of the District's Environmental Protection Officer.
  - 3.3 Submit a "Sediment and Erosion Plan" to the District's Environmental Protection Officer for approval, which the Owner shall comply with and be responsible for maintaining, repairing and implementing the sediment control measures.
  - 3.4 Enter into a "Works and Services Agreement" (WSA) to ensure installation of *site* *r* *s* to the satisfaction of the District's Manager of Development Engineering. The WSA will require engineering civil drawings detailing works, including but not limited to:
    - (a) storm water management measures;
    - (b) site service connections;
    - (c) new boulevard plan along the frontage of the site including curbs, sidewalk and grading plan; and
    - (d) repaving along the frontage of the Lands,which must be submitted for acceptance, and security provided for the due and property completion of the engineering works, all to the satisfaction of the District's Manager of Land Development.
  - 3.5 Identify the location and number of accessible units and specify the accessibility measures to be installed in each unit, to the satisfaction of the District's Director of Planning and Development Services (or designate).
  - 3.6 Provide a letter, and requisite information, to the Director of Planning and Development Services that outlines how conditions of the development covenant (required through the rezoning process) have been met.
4. Prior to the issuance of a building permit and as security for the due and proper completion of the landscaping works ("Landscaping Works") as set forth in Section 2.2 of this Development Permit, the Owner shall:
  - 4.1 Provide, to the District's Manager of Land Development, a cost estimate ("Cost Estimate") for the on-site Landscaping Works to be installed.

4.2 Provide, to the District's Manager of Land Development, a landscape deposit ("Landscape Deposit") in the amount determined by the Cost Estimate, in the form of cash or unconditional, irrevocable auto-renewing letter of credit issued by a Canadian chartered bank or credit union to ensure the due and proper completion of the Landscaping Works.

4.3 Release of the Landscape Deposit:

(a) Following installation of the Landscape Works and upon receipt of a certified letter or report by a Landscape Architect in good standing with the British Columbia Society of Landscape Architects to the District stating that:

- a. the Landscaping Works have been installed substantially in accordance with Schedule A; and
- b. any variations that may have been undertaken to the Landscaping Works are clearly identified, including but not limited to:
  - i. any adjustments to retaining walls,
  - ii. changes to the mixture or sizes of any plant materials or trees,
  - iii. completion of any off-site or boulevard works,
  - iv. any areas that received alternative treatment,
  - v. any paving changes, or
  - vi. any other additional or omitted plantings or alterations,

together with a clear rationale and explanation thereof and stating

- c. that a final review with the landscape contractor or consultant of record has been completed, including provision of the date when this final review was completed on,
- d. whether there are any outstanding Landscape Works which are outstanding or which need attention, and
- e. notwithstanding outstanding works in 5.2(a)(d) above, that the Landscaping Works are complete,

then District will release 75% of the initial value of the Landscape Deposit. The remaining 25% of the initial value of the Landscape Deposit shall be retained by the District as a warranty deposit (the "Warranty Deposit") to ensure successful installation of the Landscaping Works.

(b) After a one-year period following certification that the Landscaping Works have been completed, and upon final certification by a Landscape Architect in good standing with the British Columbia Society of Landscape Architects that the Landscaping Works are successful, the District will release the Warranty Deposit.

6. Prior to Occupancy:

6.1 The applicant must submit documentation demonstrating that the “as-built” development complies with all requirements of this development permit. Any variations must be clearly identified with a rationale and explanation noting that planning staff review and approval may be needed for variations prior to final occupancy.

7. This Development Permit lapses if the work authorized herein is not commenced within 24 months of the date this permit is issued.

THE COUNCIL OF WEST VANCOUVER APPROVED THIS PERMIT BY RESOLUTION PASSED ON \_\_\_\_\_.

\_\_\_\_\_  
MAYOR

\_\_\_\_\_  
CORPORATE OFFICER

THE REQUIREMENTS AND CONDITIONS UPON WHICH THIS PERMIT IS ISSUED ARE ACKNOWLEDGED AND AGREED TO BY THE CURRENT OWNER. IT IS UNDERSTOOD:

- THAT OTHER PERMITS / APPROVALS MAY BE REQUIRED INCLUDING PERMITS / APPROVALS FOR BUILDING CONSTRUCTION, SOIL AND ROCK REMOVAL OR DEPOSIT, BOULEVARD WORKS, AND SUBDIVISION; AND
- THE DEVELOPMENT MUST ATTAIN REQUIREMENTS OF THE BC BUILDING CODE AND ANY VARIANCES TO THE ZONING BYLAW ARE THE RESPONSIBILITY OF THE OWNER AND MUST BE RECTIFIED AT THE BUILDING PERMIT STAGE.

FOR THE PURPOSES OF SECTION 7, THIS PERMIT IS ISSUED ON \_\_\_\_\_.

Schedules:

- A - Architectural Drawings (DA Architects + Planners dated June 10, 2022).
- B - Landscape Drawings (Vaughan Landscape Planning & Design and DA Architects + Planners dated June 10, 2022)

END OF DEVELOPMENT PERMIT 21-185

# Clyde Ave. Microunits

671, 685, 693 Clyde Ave & 694 Duchess Ave.,  
West Vancouver

## Schedule "A"

Issued for: Development Permit  
Date: 2022-06-10



### DRAWING LIST:

Executive Summary	A201 Site Plan
Development Proposal	A301 Parking Plan P1
Statistical Summary	A302 Level 1 Floor Plan
Existing Context: Neighbourhood	A303 Levels 2-4 Floor Plan
Existing Context: Development	A304 Level 5 Floor Plan
Existing Context: Photos	A305 Level 6 Floor Plan
Target Market	A306 Rooftop Amenity Plan
Microunit Concept - Typical Units	A320 Exterior Elevations
3D Views - Context	A321 Exterior Elevations
3D Views - View Studies	A322 Exterior Elevations
Design Concept	A330 Building Sections
Design Rationale - Precedent Imagery	A331 Building Sections
3D Views - Proposal	A335 Partial Sections
Material Board	A336 Partial Sections
Off Ramp Glare Screening	
Off Ramp CPTED Measures	
Space Saving Features / Accessibility	
Sustainability	
Amenity Areas - Precedent Imagery	
Shadow Studies	
Site Sections	



PARK ROYAL

# SUMMARY

## Clyde Avenue Microunits Executive Summary

This proposal presented for the consideration of a Development Permit and Rezoning submission responds to the need for more diverse and affordable rental housing in West Vancouver. While the notion of for rent micro-units is new to West Vancouver, this type of housing has provided comfortably smart living, at an attainable price range, to young professionals, frontline and service workers in communities around the world.

The introduction of micro-units is proposed in this particular location given the proximity to public and active transportation, as well as close proximity to amenities, shopping and services. It is a creative solution to help West Vancouver address some of the challenges outlined in the November 2020 West Vancouver Housing Report. The Report states in part “labour force participation rates have been on the decline, and close to three-quarters of the labour force **commutes** into the municipality for work. Feedback from engagement indicates **the cost of housing is increasingly out-of-reach and employers are struggling to recruit and retain staff. Mid-market rental and affordable home ownership should be strongly considered to help meet the needs of the local labour force.**”

The Clyde Avenue neighbourhood consists of market housing, rental housing, senior living, offices and services. The introduction of micro-unit rental accommodation is not only complimentary to the existing uses, but would also create an attractive entrance feature, as a gateway to the existing compact cluster of buildings.

Park Royal is excited about working with West Vancouver Council, staff and the community to bring this innovative and forward-looking style of rental accommodation to life.



# PROPOSAL

## Existing & Future Context

The development site is located on Clyde Avenue off Taylor Way and is characterized by the following features:

- Comprised of three lots fronting Clyde Avenue, as well as a single lot fronting Duchess Avenue.
- Has never been developed and is currently serving as an ancillary parking lot for Park Royal North commercial.
- To the east is an existing 5-storey seniors housing complex, to the south is an existing 6 level parking structure (34' 8" high)
- To the north is existing 2-storey single family residential housing.
- To the west is a northbound exit ramp that allows the vehicular traffic from Park Royal North to exit onto Taylor Way.
- The north edge of the site is characterized by a steep embankment with mature vegetation.
- The parking structure to the south is zoned for that use and currently there are no plans for redevelopment as it was excluded from the Marine Drive Local Area Plan.
- Further north, the 657 Marine Dr. development has been approved at Marine Dr. & Taylor Way intersection including buildings from 4-8 storeys high.

## Project Description

The proposal conforms to the District's current OCP as it relates to Clyde Avenue. However, given the current zoning the property needs to be rezoned in order to allow construction of 100% Rental Micro-unit Housing as follows:

- Rental Micro-unit Housing development at the corner of Clyde Ave. and Taylor Way
- 6-storey housing component with rooftop amenity
- FAR: 2.04 (1.75 base + 0.25 Rental bonus + 0.04 Step 3 Compliance)
- FAR Floor Area: 8,271.24 sm (89,031 sf)

- Unit Type: Rental Micro Units
- Number of Units: 201
- Unit Mix:
  - 87% (174) Move-In Ready Micro-units (350-378 sf)
  - 13% (27) Adaptable Level 2 Micro-units (452-474 sf)
- Parking: 51 stalls at 0.25 stalls per unit
- Bicycle Parking: 306 stalls at 1.5 stall per unit

## Project Design Rationale

The proposed development is located in the transition area between the 4-8 storey density at Marine Drive and the 2-storey single family neighbourhood to the north. The project design is based on the following major considerations:

- 6-storey building massing mediates between the larger scale to the south and the lower scale to the north, as well as the 5-storey seniors housing to the east.
- The building is sited on the three lots fronting Clyde Avenue at the entrance to this neighbourhood.
- The northern lot off Duchess Avenue will not be developed as part of this proposal and provides an opportunity for outdoor green space or another amenity accessible to the public.
- The U-shape building massing configuration creates a south-facing courtyard that is buffered from the traffic at Taylor Way.
- The rental housing comprises a 6-storey massing block with a rooftop amenity garden.
- The project will provide 201 micro-units ranging between 350-474 sf in size, with all the units being move-in ready.
- The outdoor amenities will include the rooftop garden, as well as the south-facing courtyard green space with outdoor fitness area.
- Adjacent to the lobby at L1 over 2000 sf of indoor amenities will include a shared indoor working IT spaces and dining / kitchen amenity.

- At each level from Level 2 to Level 6 there will be a lounge / coworking area off the elevator lobby.
- All vehicular and bicycle parking, as well as the garbage / recycling facilities are provided on a single level of underground parking at P1.
- The bicycle facilities will include a bicycle repair area.
- Provisions for vehicular and bicycle electrical charging will also be included.
- The development will increase the supply of affordable rental housing in the area through provision of move-in ready micro-units.
- Located in close proximity to the Marine Drive / Taylor Way node, the residents will have access to public transportation, as well as active transportation modes.

## Compliance with OCP policies

The proposed development adheres to the intent of the OCP at Clyde Avenue East of Taylor Way (BF-D 2) with the following features:

- Provision of move-in ready rental micro-unit housing with close proximity to transit / active transportation
- High performance building with Energy Step Code: Step 3 with LCES or Step 4 compliance
- Electrical charging for cars and bicycles
- Extensive indoor / outdoor amenities
- Provision of public green space at Taylor Way / Duchess
- High quality windows to mitigate traffic noise from Taylor Way
- Utilizing reclaimed wood from the original 1950 Park Royal North buildout
- Landscaped courtyard with pedestrian-friendly design features at Clyde Avenue
- Articulated building facades to better relate to the existing 2-5 storey neighbourhood scale
- Gateway articulation at Clyde Ave / Taylor Way through prominent, highly-articulated stair towers

# STATISTICAL SUMMARY

Current Zoning: CD30  
 Lot 38, 39, 40 RS3  
 Lot 25 RS3  
 Lot width (at Clyde Ave) 67.9 m 222.77 ft

Total Project Site Area: 4,054.53 s.m. 43,643 s.f.  
 Allowable FAR Ratio (w/ Step 3): 2.04  
 Allowable FAR Area: 8,271.24 s.m. 89,031 s.f.  
 Proposed FAR Ratio: 2.04  
 Proposed FAR Area: 8,271.24 s.m. 89,031 s.f.  
 Proposed Height: 6 storeys 21.6m (70.87 ft)  
 Proposed Units: 201

Front yard Setback: 3.7 m  
 Side Yard Setback: 3.7 m  
 Rear Yard Setback: 4.5 m  
 Site Coverage: 36%

## Development Area Summary

	Gross Floor Area		FAR Exclusions		FAR Area		Units
	s.m.	s.f.	s.m.	s.f.	s.m.	s.f.	
Level P1	2,418.48	26,032	2,418.48	26,032	-	-	
Level 1	1,338.07	14,403	283.71	3,054	1,054.36	11,349	26
Level 2	1,468.57	15,808	25.19	271	1,443.38	15,536	35
Level 3	1,468.57	15,808	25.19	271	1,443.38	15,536	35
Level 4	1,468.57	15,808	25.19	271	1,443.38	15,536	35
Level 5	1,468.57	15,808	25.19	271	1,443.38	15,536	35
Level 6	1,468.57	15,808	25.19	271	1,443.38	15,536	35
Level 7 - Rooftop Amenity	216.53	2,331	216.53	2,331	-	-	
Total GFA(above grade):	6,697.43	95,771			8,271.24	89,031	201

**FAR Area Exclusions:** entrance lobby area, recreation rooms, common area in basement used for hallways, elevator shafts, stairwells, mechanical / service rooms, storage spaces, parking & loading areas.

## Parking Requirements Summary

	Stalls / Unit	# of Units	Required	Provided
Residential Parking	0.20	201	40	40
Res. Visitor Parking	0.05	201	10	10
Total:	0.25	201	50	50

## Parking Stall Type Summary

	Regular	Small Car	Accessible	Total
Residential Parking	31	5	4	40
Res. Visitor Parking	6	3	1	10
Total:	37	8	5	50

## Bicycle Parking

	Stalls/Unit	# of Units	Required	Provided
Short-term Spaces	0.1	201	20	20
Horizontal Spaces				130
Vertical Spaces (35% max)			106	88
Stacked Spaces			90	85
Total:	1.5	201	302	303

43%  
29%  
28%

**Bicycle Parking:** Long-term Spaces: 1.5 stalls per unit required per bylaw for apartment residential.  
 Short-term Spaces: 0.1 stalls per unit, see TIA for details

## Unit Mix

	Type A1 Microunit (350 sf)	Type A2 Microunit (362 sf)	Type B1 Microunit (350 sf)	Type C Microunit (378 sf)	Type G Adaptable Microunit (496 sf)	Type E Adaptable Microunit (466 sf)	Type F Adaptable Microunit (497 sf)	Total
Level 1	22	0	1	1	0	2	0	26
Level 2	25	1	2	2	2	2	1	35
Level 3	25	1	2	2	2	2	1	35
Level 4	25	1	2	2	2	2	1	35
Level 5	25	1	2	2	2	2	1	35
Level 6	25	1	2	2	2	2	1	35
Total:	147	5	11	11	10	12	5	201
% Mix:	73%	2%	5%	5%	5%	6%	2%	100%
Total Microunits:	174				27			
Total % Microunits:	79%				13%			

# EXISTING CONTEXT: NEIGHBOURHOOD



# EXISTING CONTEXT: DEVELOPMENT



# EXISTING CONTEXT: PHOTOS



# EXISTING CONTEXT: PHOTOS



# TARGET MARKET: Workplace & Young Professionals Housing



Medical Support Staff



Service Workers



Retail workers



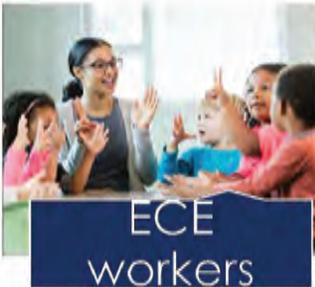
Tech workers



Library workers



Film workers



ECE workers



Teachers



Senior care

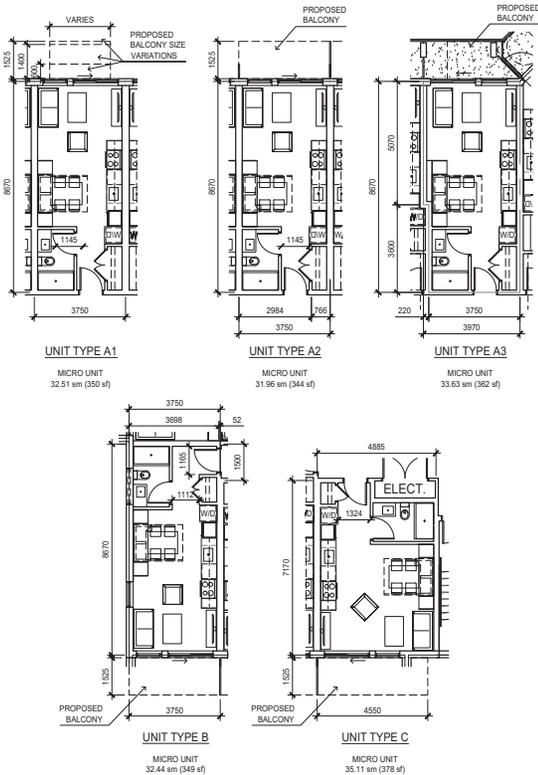


Anyone who works long hours

# MICRO UNIT CONCEPT

## TYPICAL MICRO UNIT

- 82% of Micro Units at 350sf provide an affordable rental price point for the residents
- Flexible space provides for all the necessities of living, dining, sleeping, relaxing and entertaining
- In-suite laundry with combination washer & dryer in each unit
- Contemporary interior design in the kitchens and bathrooms with energy efficient appliances
- High performance building with increased thermal comfort
- The amenity package is designed to complement the efficient suite layouts with over 2,000 sf of the indoor amenity area, as well as the expansive rooftop outdoor amenity garden



### STORAGE

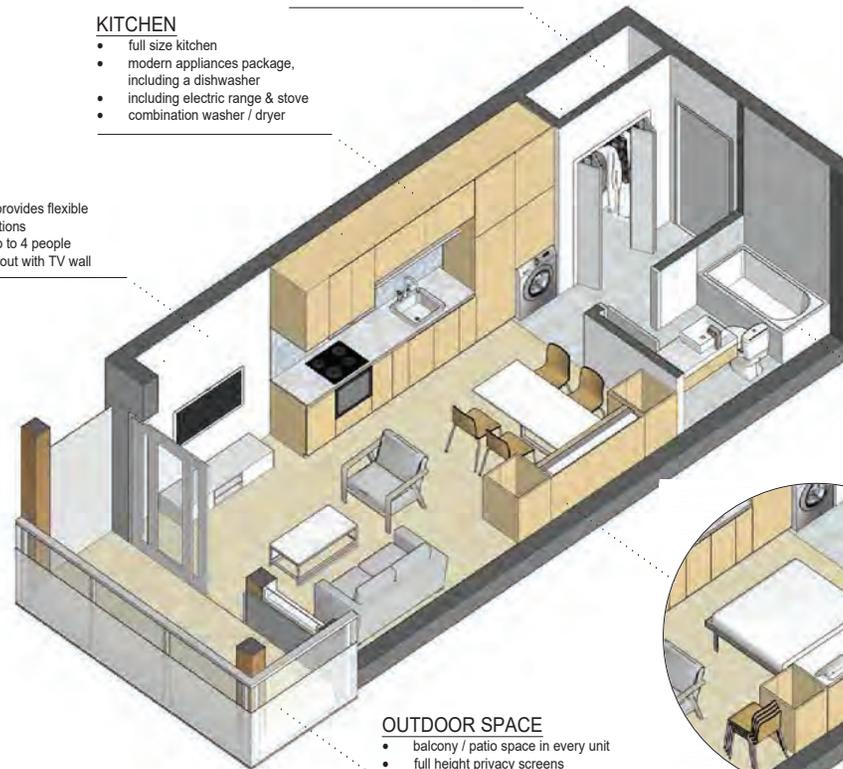
- maximized storage space
- large entry closet
- built-in storage above washing machine & either side of dining

### KITCHEN

- full size kitchen
- modern appliances package, including a dishwasher
- including electric range & stove
- combination washer / dryer

### LIVING

- living space provides flexible furnishing options
- seating for up to 4 people
- functional layout with TV wall



### BATHROOM

- full size bathroom
- full size bath tub or shower
- modern fixtures & finishes

### DINING / WORKING & SLEEPING / RELAXING

- flexible built-in furniture
- fold down dining / work table
- fold down murphy bed
- storage closets on either side

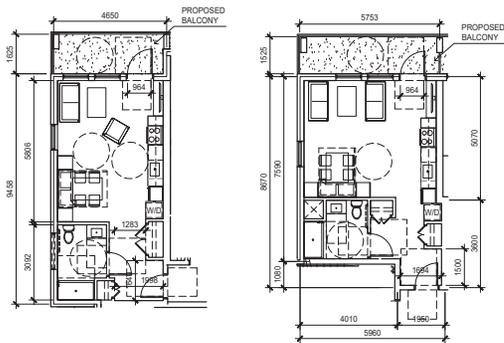
### OUTDOOR SPACE

- balcony / patio space in every unit
- full height privacy screens

# ADAPTABLE MICROUNIT CONCEPT

## TYPICAL ADAPTABLE MICRO UNIT

Adaptable design creates liveable residences for a wider range of persons than current building codes require. By considering design features that can be easily and inexpensively incorporated at a future time, adaptable design also allows flexibility for residents whose needs may change over time. 13% of units in this development are designed to meet the Level 2 Adaptability Standard per the District guidelines.

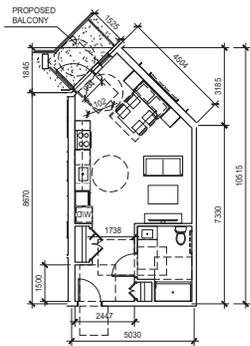


UNIT TYPE E

ADAPTABLE (LEVEL 2)  
MICRO UNIT  
43.3 sqm (466 sf)

UNIT TYPE F

ADAPTABLE (LEVEL 2)  
MICRO UNIT  
46.55 sqm (507 sf)



UNIT TYPE G

ADAPTABLE (LEVEL 2)  
MICRO UNIT  
45.11 sqm (486 sf)

### KITCHEN

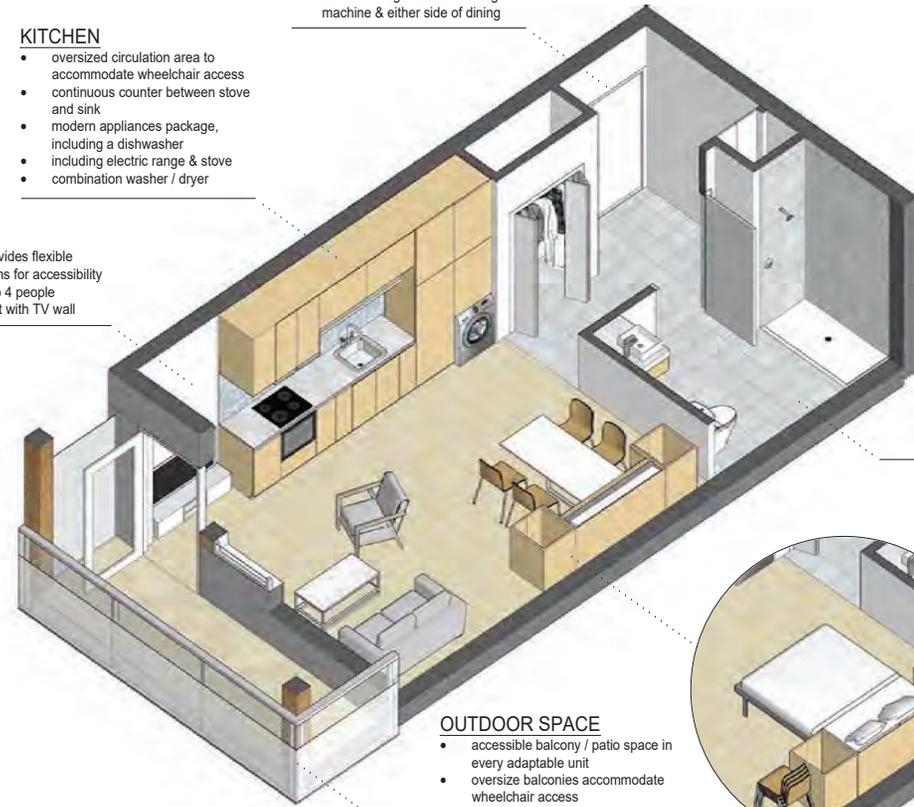
- oversized circulation area to accommodate wheelchair access
- continuous counter between stove and sink
- modern appliances package, including a dishwasher
- including electric range & stove
- combination washer / dryer

### LIVING

- living space provides flexible furnishing options for accessibility
- seating for up to 4 people
- functional layout with TV wall

### ENTRY / STORAGE

- oversize entry to accommodate wheelchair turning radius
- maximized storage space
- large entry closet
- built-in storage above washing machine & either side of dining



### BATHROOM

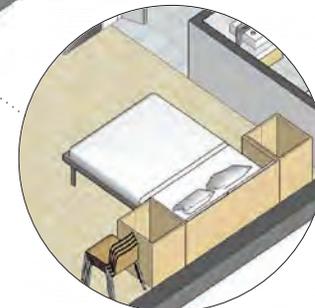
- oversized bathroom to accommodate future accessibility upgrades
- barrier free shower
- bathroom accommodates wheelchair access

### DINING / WORKING & SLEEPING / RELAXING

- flexible built-in furniture
- fold down dining / work table
- fold down murphy bed
- storage closets on either side

### OUTDOOR SPACE

- accessible balcony / patio space in every adaptable unit
- oversize balconies accommodate wheelchair access
- full height privacy screens

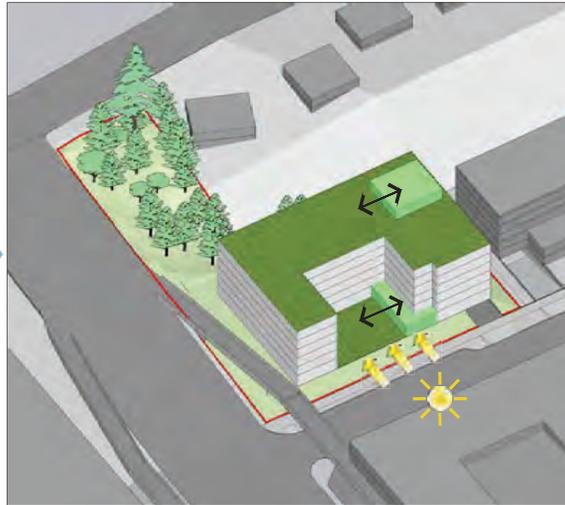


# DESIGN CONCEPT

1. A 6-storey building is placed on the three south lots, allowing the north lot to become a public outdoor amenity.



2. Outdoor amenity spaces are created for the residents at the south facing courtyard on the main level and at the rooftop garden.



3. The building form is articulated through a series of 4-storey volumes that relate to the existing 2-5 storey context.



# DESIGN RATIONALE

## 1. Development Permit Guidelines



## 2. Existing Neighbourhood Context



## 3. Emerging Character of Park Royal



## 4. West Coast Contemporary Style



# DESIGN RATIONALE

## 1. Development Permit Guidelines

### Design Principles:

#### II. BUILDING DESIGN

---

- a. Encourage high performance or “green” buildings.
- b. Situate buildings within a landscaped setting..
- c. Step back from the street, floors above the second storey, , and introduce planters, exterior balconies, and building articulation to create interest and complement the landscaped character.
- d. Design pedestrian scale ground floor spaces oriented to Clyde Avenue.
- e. Recognize the potential contribution of building massing, design and character to the gateway image of West Vancouver.
- f. Orient building entrances to face the street with high visibility and pedestrian accessibility.
- g. Consider the impact on privacy of adjacent homes on the north side of Clyde Avenue. Outdoor patios and balconies should generally be south facing.
- h. Encourage the use of sloped roofs or components.
- i. Site and screen loading areas, mechanical equipment and garbage bins, within the building, to minimize visual and acoustical impacts on adjacent properties and the streetscape.
- j. Provide signage compatible with a mixed-use area and designed to fit and complement buildings.

#### III. LANDSCAPE DESIGN

---

- a. Create an overall park like character for the landscape treatment.
- b. Encourage public access connections between Capilano River and Taylor Way through the use of landscape treatments.
- c. Retain the bank on the north side of Clyde Avenue in a forested, natural state to provide separation of mixed commercial / residential development from the low density residential area to the north. A geo-technical report addressing the potential impact of a proposed development on the stability of the bank and groundwater flows should be provided.
- d. Maintain and restore the edge adjacent to the Capilano River walkway in a natural state. Impervious paved surfaces, fencing or walls directly adjacent to the walkway should be minimized.
- e. Provide useable common open space.
- f. Locate the common area landscaped open space in a courtyard, side yard or rear yard depending on the building configuration.
- g. Incorporate landscape elements in and around the entire site with planting beds, planters, hanging baskets, and pots located at the ground level.
- h. Provide a cohesive pattern of concrete or brick paving for pedestrian walkways.
- i. Minimize retaining walls visible from the street. Where unavoidable, retaining walls should be constructed or faced with natural materials such as rock or heavily screened with landscaping.
- j. Design landscaping to minimize potential view impacts.
- k. Minimize exterior lighting glare and light spill to surrounding properties.

# DESIGN RATIONALE

## 2. Existing Neighbourhood Context

### Design Principles:

- Top / Middle / Base Articulation
- Use of Brick at the base
- Neutral / Pastel Colour Palette
- Horizontal Expression
- "Floating Roof"



Amica Seniors Housing



Water's Edge



Heritage Building



Watermark Apartments



# DESIGN RATIONALE

## 3. Emerging Character of Park Royal

### Design Principles:

- Wood expression as a feature
- Use of dark, rich brick at the base
- Contrasting light and dark elements
- Warmth of wood
- Accentuated Horizontals



Executive Parc Residential



Gateway Residences



Arthur Erickson Place



# DESIGN RATIONALE

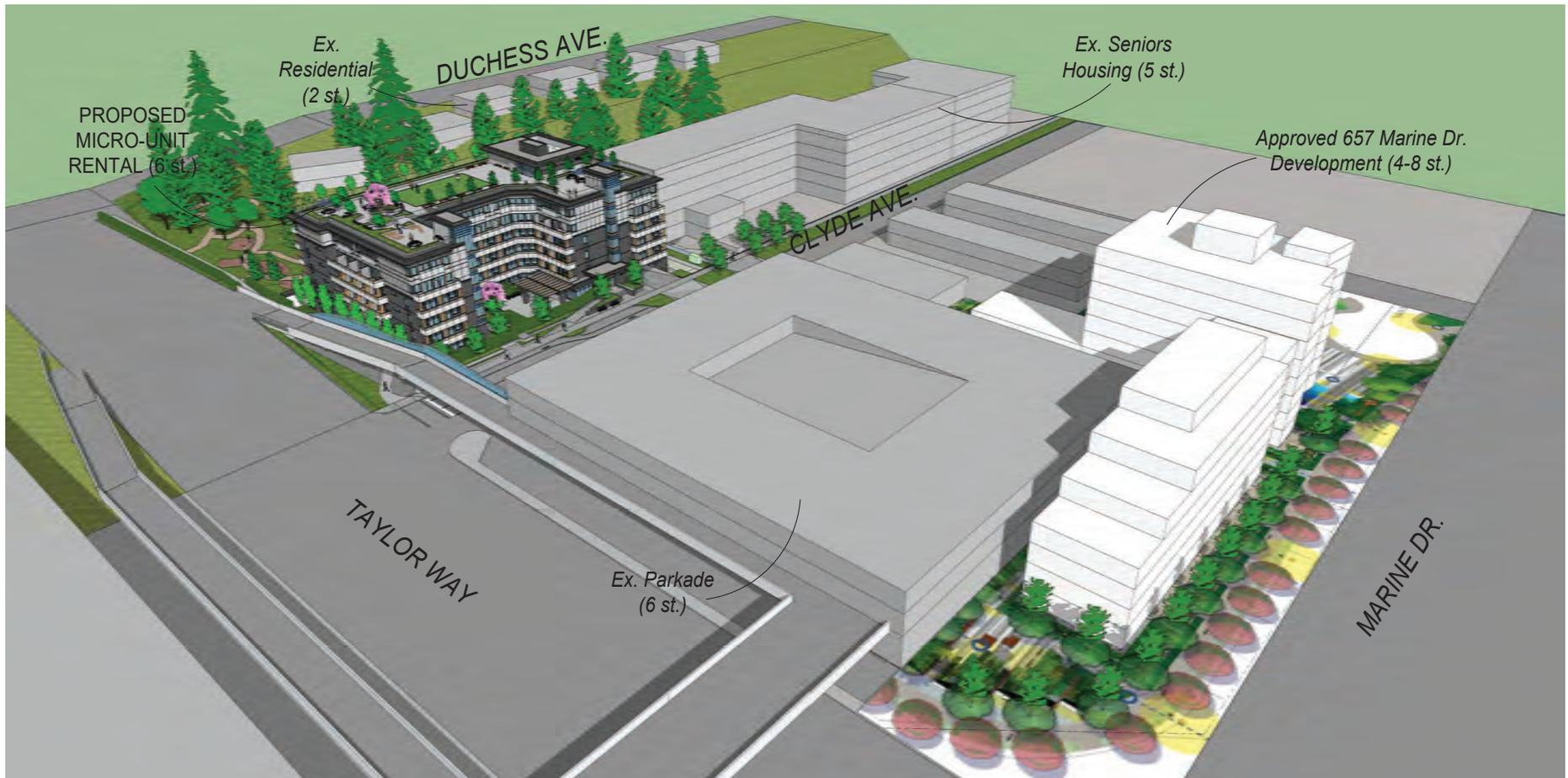
## 4. West Coast Contemporary Style

Design Principles:

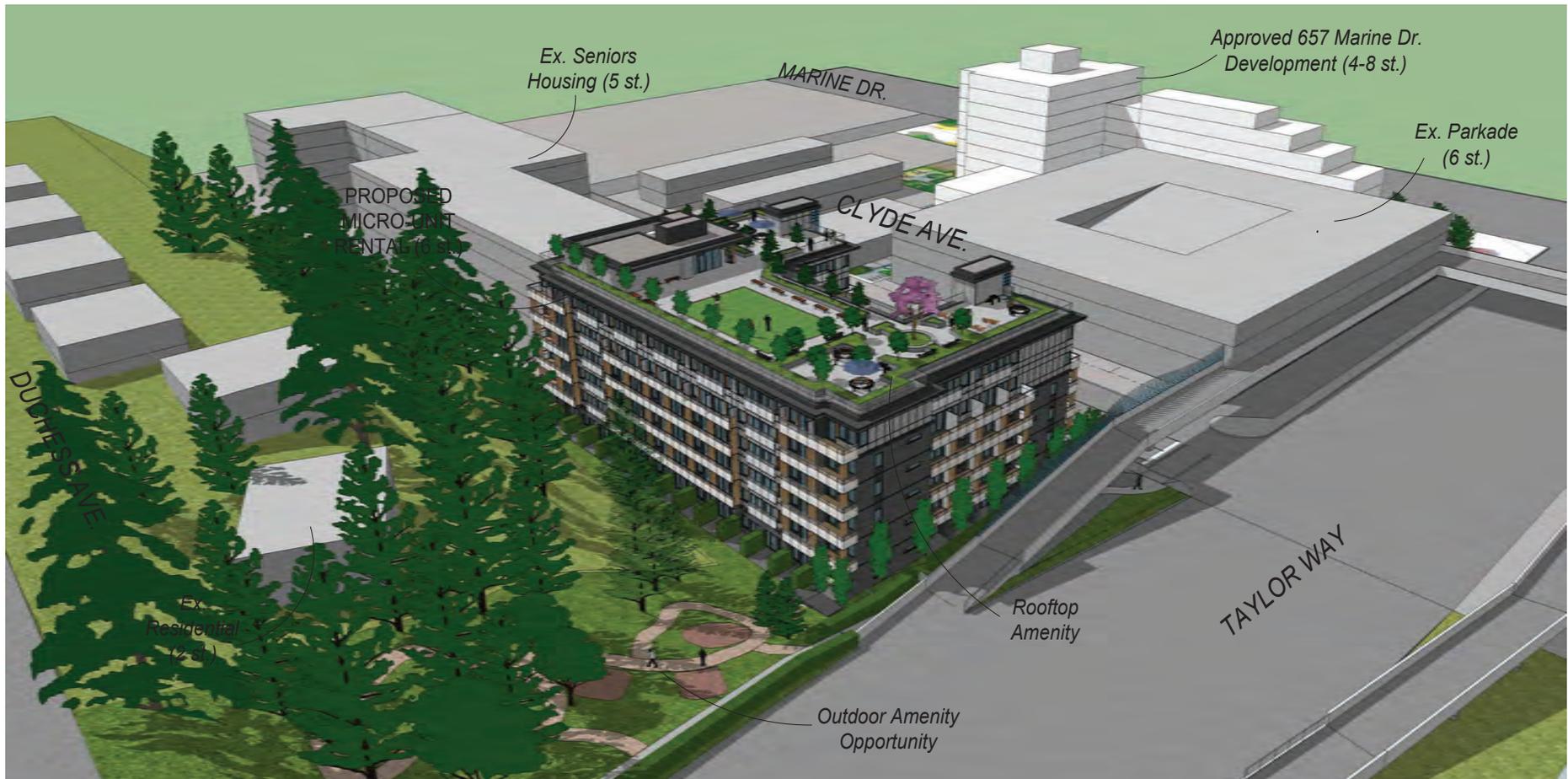
- Use of Local Natural Materials
- Building in landscape
- Openness to the Outdoors
- Muted colour palette
- Horizontal Expression



# SOUTH-WEST CORNER - FUTURE CONTEXT



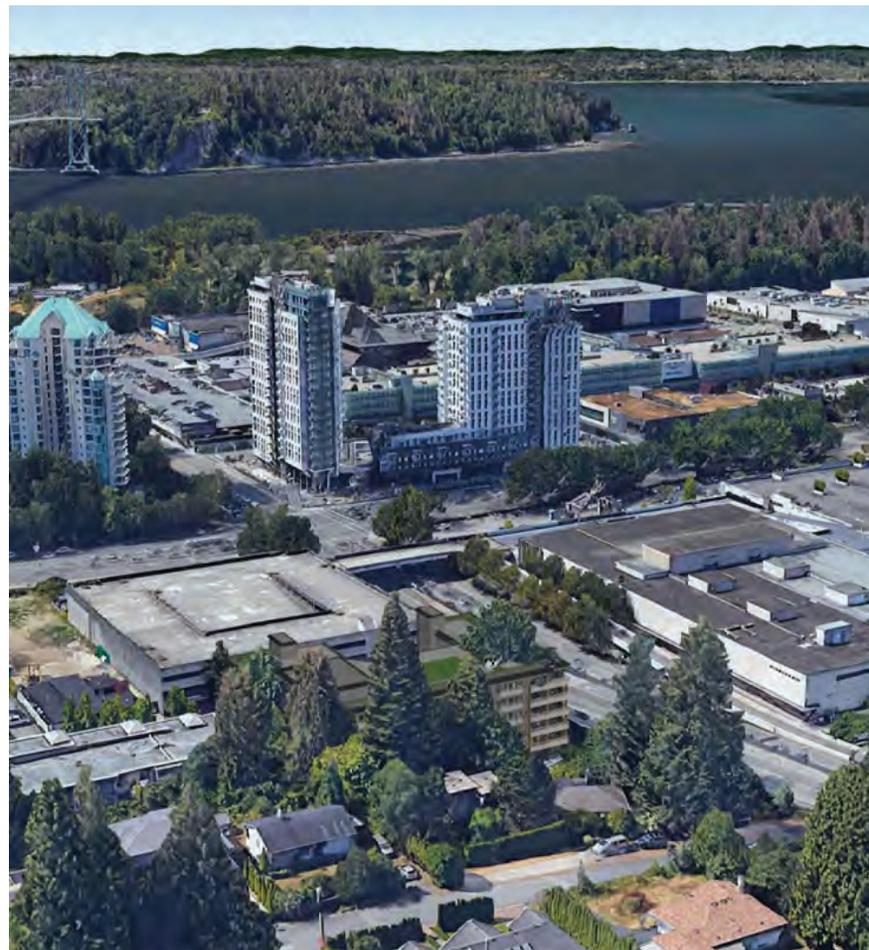
# NORTH-WEST CORNER - FUTURE CONTEXT



# AERIAL VIEWS



① AERIAL VIEW - LOOKING NORTH-EAST



② AERIAL VIEW - LOOKING SOUTH-WEST

# WEST ELEVATION - TAYLOR WAY



# SOUTH ELEVATION



# SOUTH COURTYARD AMENITY



# SOUTH COURTYARD AMENITY



# SOUTH/ EAST ELEVATION



# MAIN ENTRANCE



# NORTH ELEVATION



# ROOFTOP AMENITY



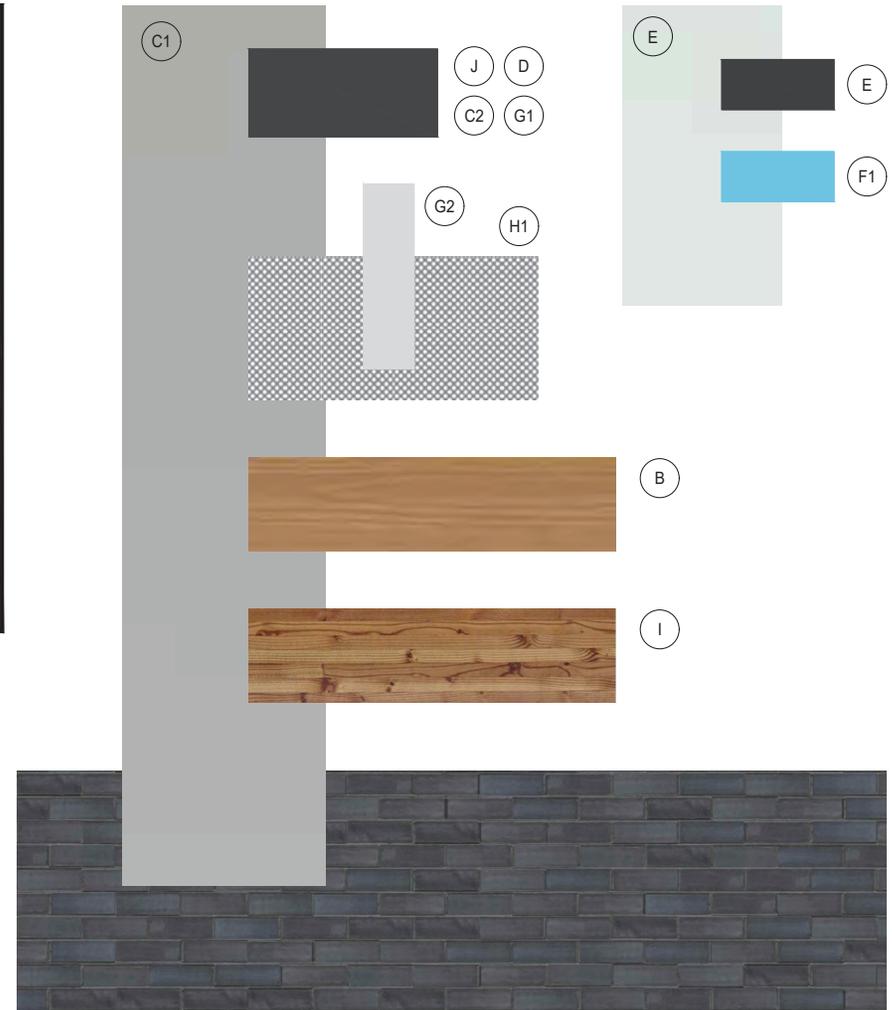
# ROOFTOP AMENITY



# MATERIAL BOARD

## MATERIALS LEGEND

- (A) BRICK CLADDING  
COLOUR: DARK GREY
- (B) CEMENTITIOUS PANEL CLADDING  
COLOUR: LIGHT BROWN
- (C) CEMENTITIOUS  
PANEL CLADDING W/ BLACK TRIMS  
C1 - COLOUR: GREY  
C2 - COLOUR: BLACK
- (D) PREFINISHED METAL FASCIA  
COLOUR: BLACK
- (E) WINDOW SYSTEM  
GLAZING: CLEAR LOW-E  
MULLIONS: BLACK
- (F) COLOURED (LAMINATED) GLASS  
F1 - SKY BLUE
- (G) PREFINISHED METAL RAILING SYSTEM  
G1 - COLOUR: LIGHT GREY
- (H) GLASS RAILING SYSTEM  
H1 - COLOUR: WHITE FRIT  
H2 - COLOUR: CLEAR GLASS
- (I) WOOD CANOPY STRUCTURE  
COLOUR: LIGHT BROWN
- (J) CEMENTITIOUS TRIM  
COLOUR: BLACK



# EXISTING OFF RAMP: GLARE SCREENING

STANDARD ANTI GLARE SCREEN:



PROPOSED DESIGN:



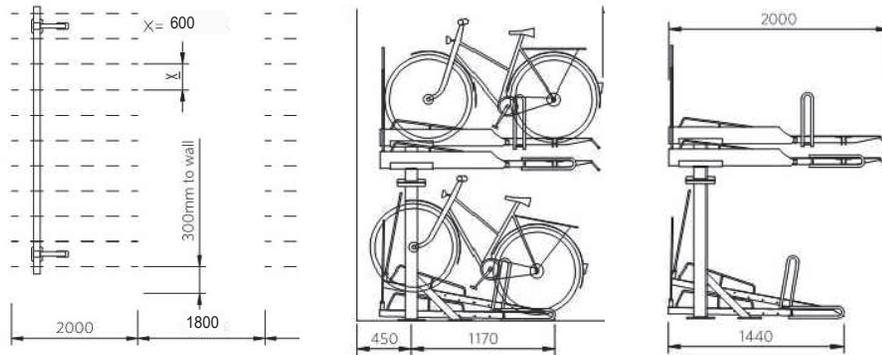
# EXISTING OFF RAMP: CPTED MEASURES



# MICRO UNIT CONCEPT

## SPACE-SAVING FEATURES

- Flexible Dining / Sleeping areas with built-in murphy beds
- Combination Washer / Dryer
- Stacked Secure Bicycle Parking Stalls:

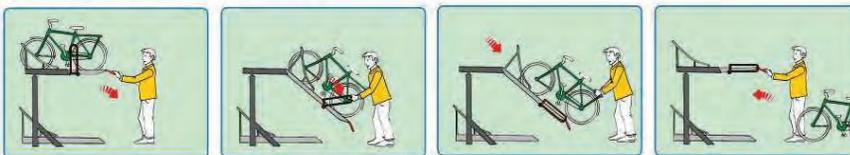


### Loading your bike on to the upper racks



1. Pull down the upper rack.
2. Lift the front wheel onto the lowered rack and push forwards.
3. Raise the locking bar and lock the bike to it.
4. Lift the rack back into the horizontal position.

### Unloading your bike from the upper racks



1. Pull down the upper rack, the bike will lower slowly towards you.
2. Unlock the bike and lower the locking bar.
3. Hold the bike and guide backwards towards you.
4. Return the upper rack to its original position.

## ACCESSIBILITY FEATURES

- 27 residential units conforming to the City of North Vancouver Level 2 Adaptable Standards.
- See ACDI comments response for additional accessibility features
- Building Entrance:
  - Accessible intercom.
  - Power operated doors.
- Exit Stairs:
  - Yellow handrails in exit stairs.
  - Yellow stair nosings.
  - Yellow tactile warning areas at the top and bottom of stairs.
- Amenity Areas:
  - Power operated door leading to all amenity rooms.
  - Doors leading from the amenity areas to the exterior shall have accessible threshold and power operated doors.
- Accessible washroom adjacent to main amenities on ground floor which will include the following:
  - Power operated entrance door.
  - BCBC compliant plumbing fixtures, handrails, washroom accessories and clearances for accessible washrooms.
  - Wall colour that achieves 70% contrast with white plumbing fixtures.
  - Floor colour contrasting with wall tile.
- Underground Parking:
  - 5 accessible parking stalls provided adjacent to elevator lobby entrance.
  - Striped access aisles beside all accessible parking stalls.
  - Striped markings extend from the accessible parking stalls to the elevator lobby entrance.
- Signage: Residential doors and amenity rooms to have raised lettering with a 70% contrast with background.
- Outdoor Areas:
  - Landscape furniture design to be accessible.
  - Level and accessible social and recreational areas.

# SUSTAINABILITY



## Enhancing Resiliency

- Shading of site hardscapes to reduce heat island effect, including green roof amenity space and landscaped central courtyard
- Potable water use reductions through installation of native landscape species and low flow plumbing fixtures to prepare for water scarcity through summer drought
- 'Park-like' public amenity and urban agriculture amenity will provide some residents the opportunity to grow their own food improving social resilience of the community
- Design for improved thermal comfort through optimized window to wall ratio, high performance glazing and exterior shading

## BUILDING SUSTAINABILITY

### Energy Performance: Step 3 with a Low Carbon Energy System

- High performance building envelope reducing heating and cooling energy consumption
- Optimized glazing placement balancing daylight penetration and thermal comfort
- Reduced reliance on fossil fuel combustion for space heating and domestic hot water with low carbon heat pump based systems

### Low Carbon Construction

- Wood frame construction supports the local low carbon building material industry, while reducing the project's embodied emissions

### Low Carbon Transportation

- Car parking will meet the minimum provision and will be equipped to support Level 2 electric vehicle charging to support the transition away from fossil fuel vehicles
- Bicycle parking and a bicycle repair station will support sustainable living and further support the mode shift away from vehicles for the dense city living lifestyle



# AMENITY PACKAGE

## Outdoor Amenity Areas

- Courtyard



- Rooftop Garden



- North Lot Green Space



## Indoor Amenity Areas (2,700 sf total)

- Level 1 (1,700 sf):
  - Social / Gathering Spaces
  - Exercise Space



- Levels 2-6 (200 sf per floor):
  - Lobby Lounge Space



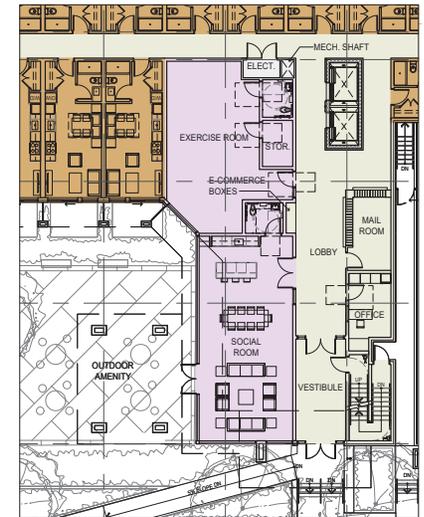
# AMENITY PACKAGE

## Indoor Amenity Areas - Level 1 (1,700 sf)

### Social / Gathering Space



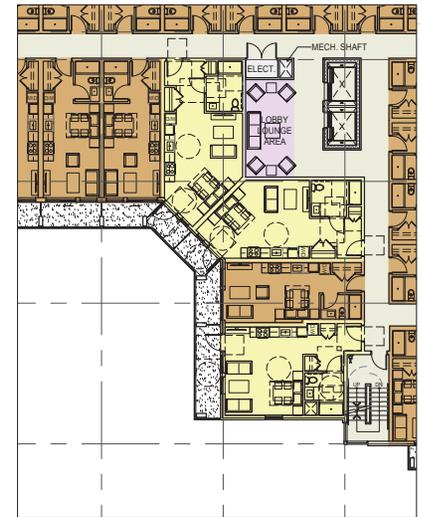
### Exercise Space



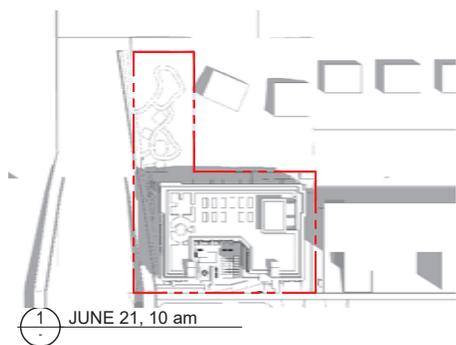
# AMENITY PACKAGE

Indoor Amenity Areas - Levels 2-6 (200sf per floor)

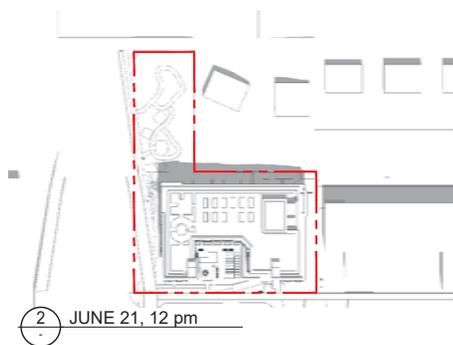
Lobby Lounge Space



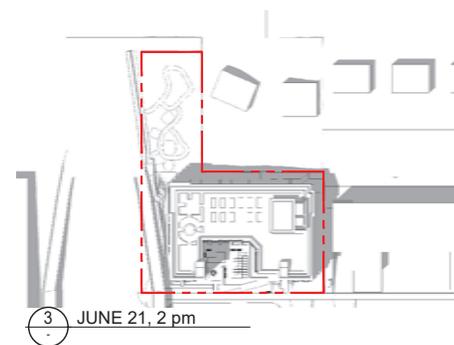
# SHADOW STUDIES



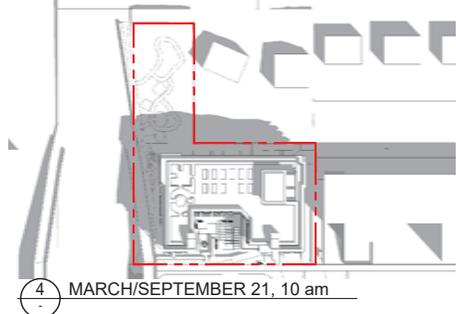
1 JUNE 21, 10 am



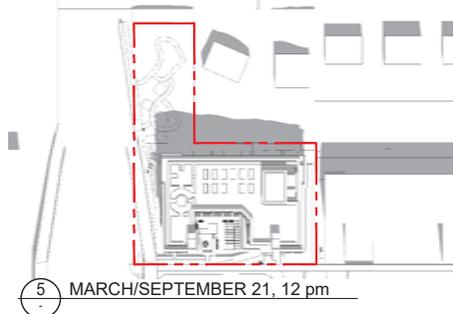
2 JUNE 21, 12 pm



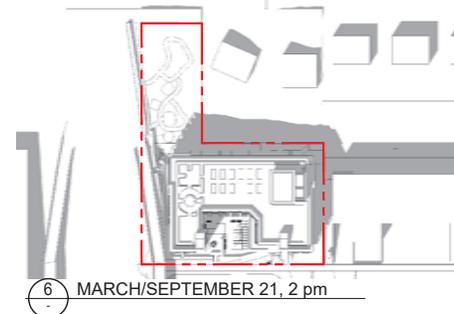
3 JUNE 21, 2 pm



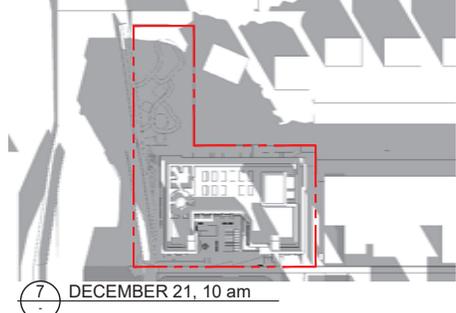
4 MARCH/SEPTEMBER 21, 10 am



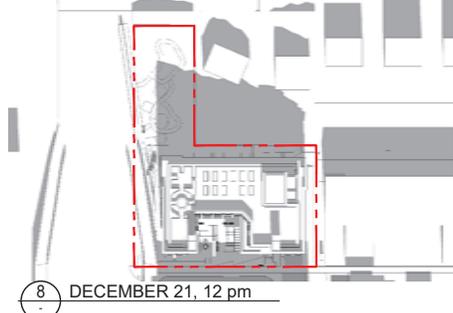
5 MARCH/SEPTEMBER 21, 12 pm



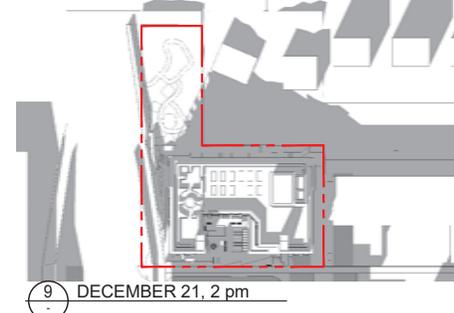
6 MARCH/SEPTEMBER 21, 2 pm



7 DECEMBER 21, 10 am

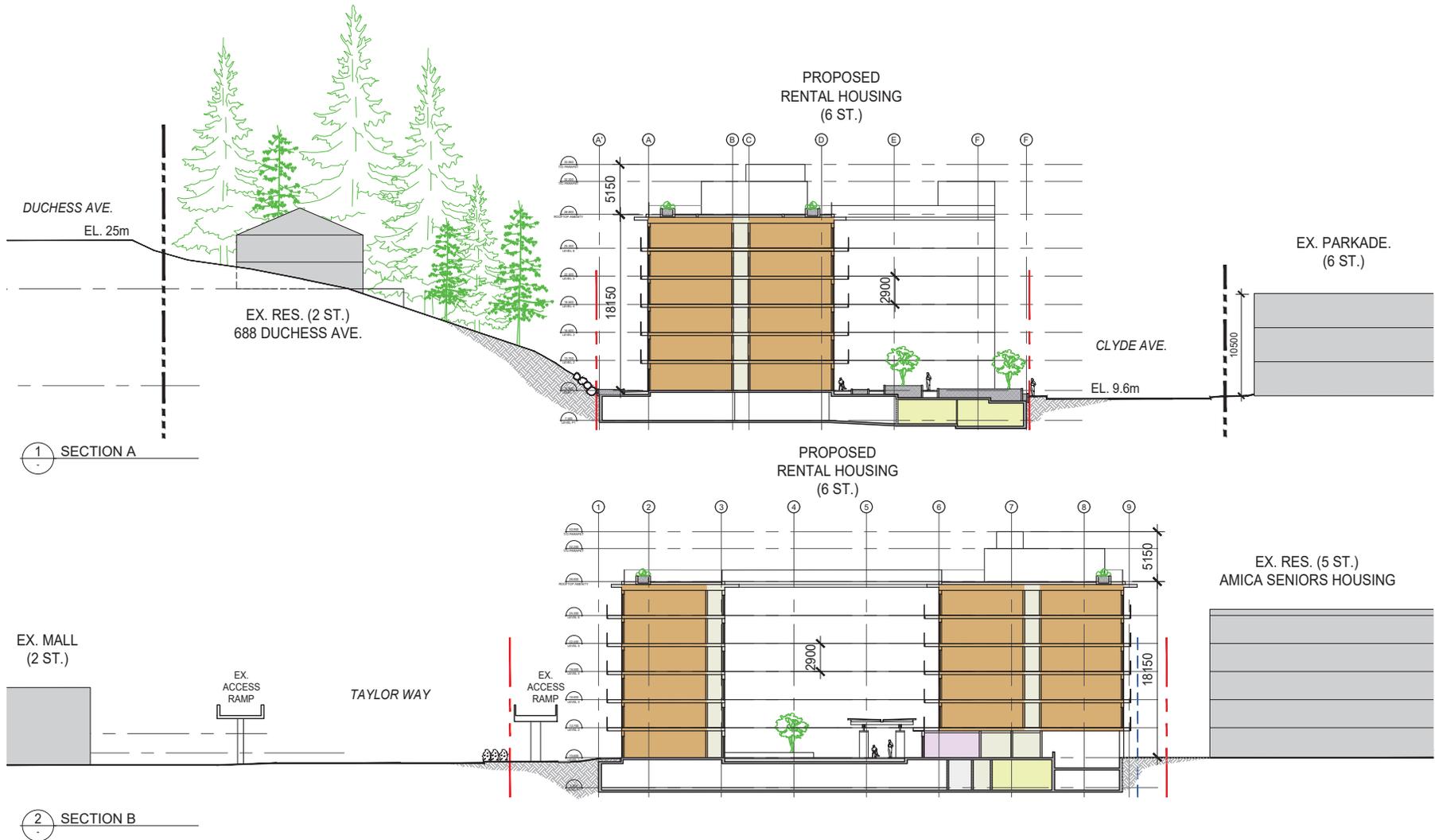


8 DECEMBER 21, 12 pm



9 DECEMBER 21, 2 pm

# SITE SECTIONS





KEY PLAN



ISSUED FOR REZONING / DP	JAN 10 2022		
ISSUED FOR REZONING / DP	DEC 21 2021		
No.	Description	Date	Dr.
Revisors: Read Up			



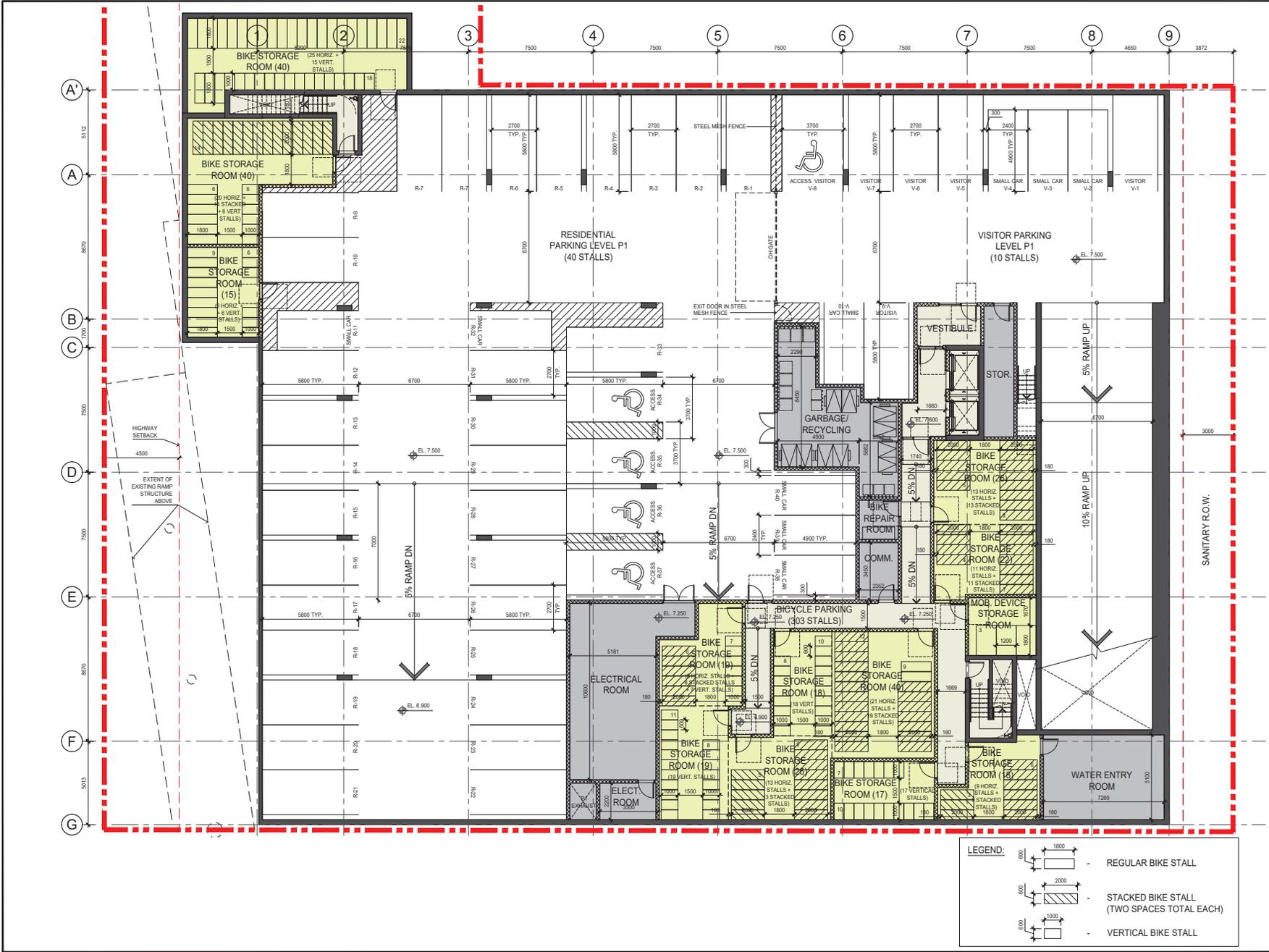
CLYDE AVE. MICRO-UNITS  
671 CLYDE AVE.  
WEST VANCOUVER, BC

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Sheet Title  
PARKING PLAN P1

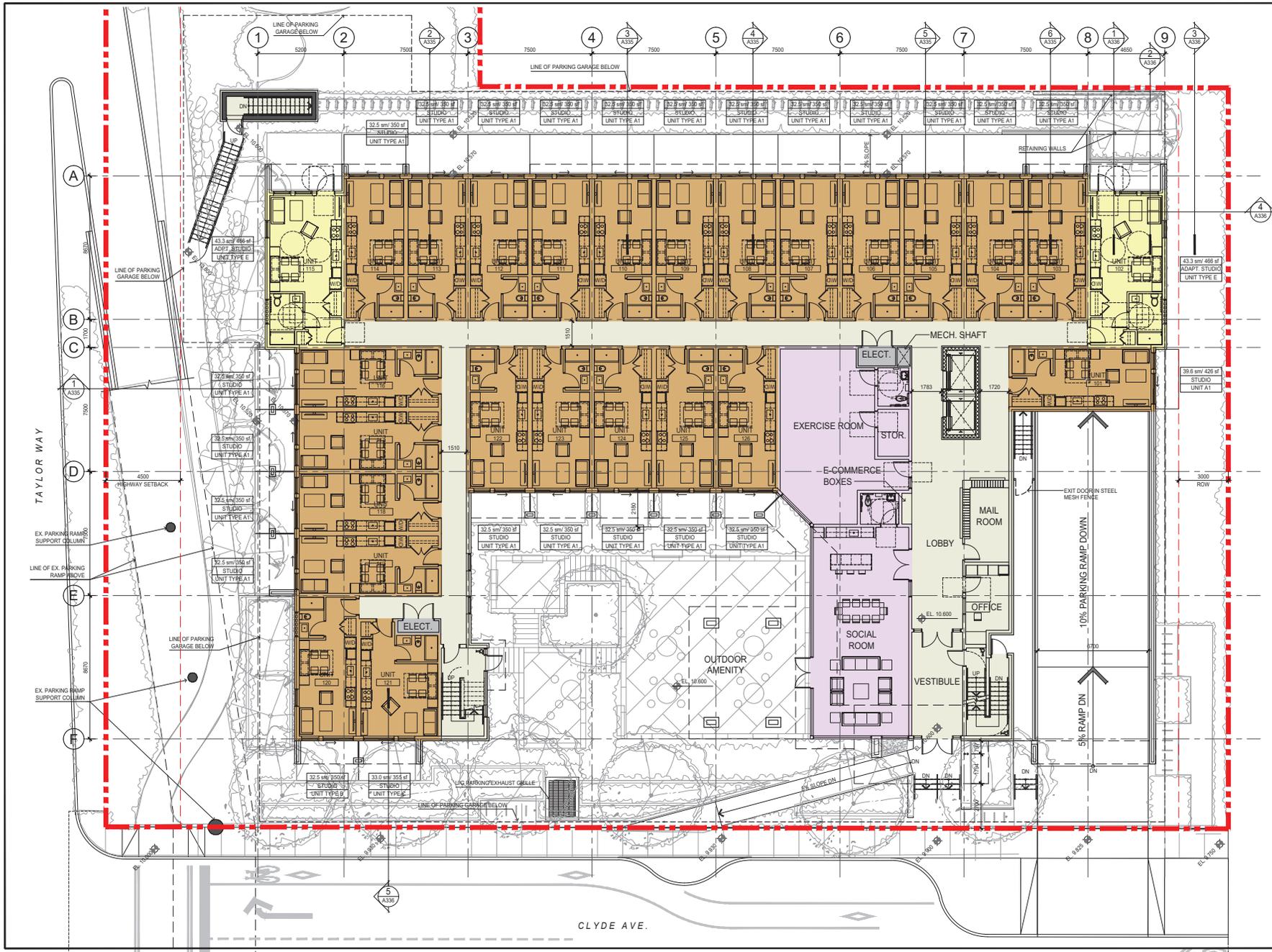
Job No.	20-14-000	Sheet No.	
Scale	1:100	Checked	ME
Drawn	MS	Approved	ME
Checked	RK	Date	2021.11.24
Approved	ME	Revision	

20-14-000\_RZ\_A301 Parking Plan P1.dwg



**LEGEND:**

- REGULAR BIKE STALL
- STACKED BIKE STALL (TWO SPACES TOTAL EACH)
- VERTICAL BIKE STALL



KEY PLAN



No.	Description	Date	Dr.
1	ISSUED FOR REZONING / DP	JAN 10 2022	
2	ISSUED FOR REZONING / DP	DEC 21 2021	
Revisors: Read Up			



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FLOOR PLAN  
LEVEL 01

Job No.	20-14-000	Sheet No.	
Scale	1:100		
Drawn	MS		
Checked	RK		A302
Approved	ME		
Date	2021.11.24	Revision	

KEY PLAN



No.	Description	Date	Dr.
1	ISSUED FOR REZONING / DP	JAN 10 2022	
2	ISSUED FOR REZONING / DP	OCT 21 2021	
Revisors: Read Up			



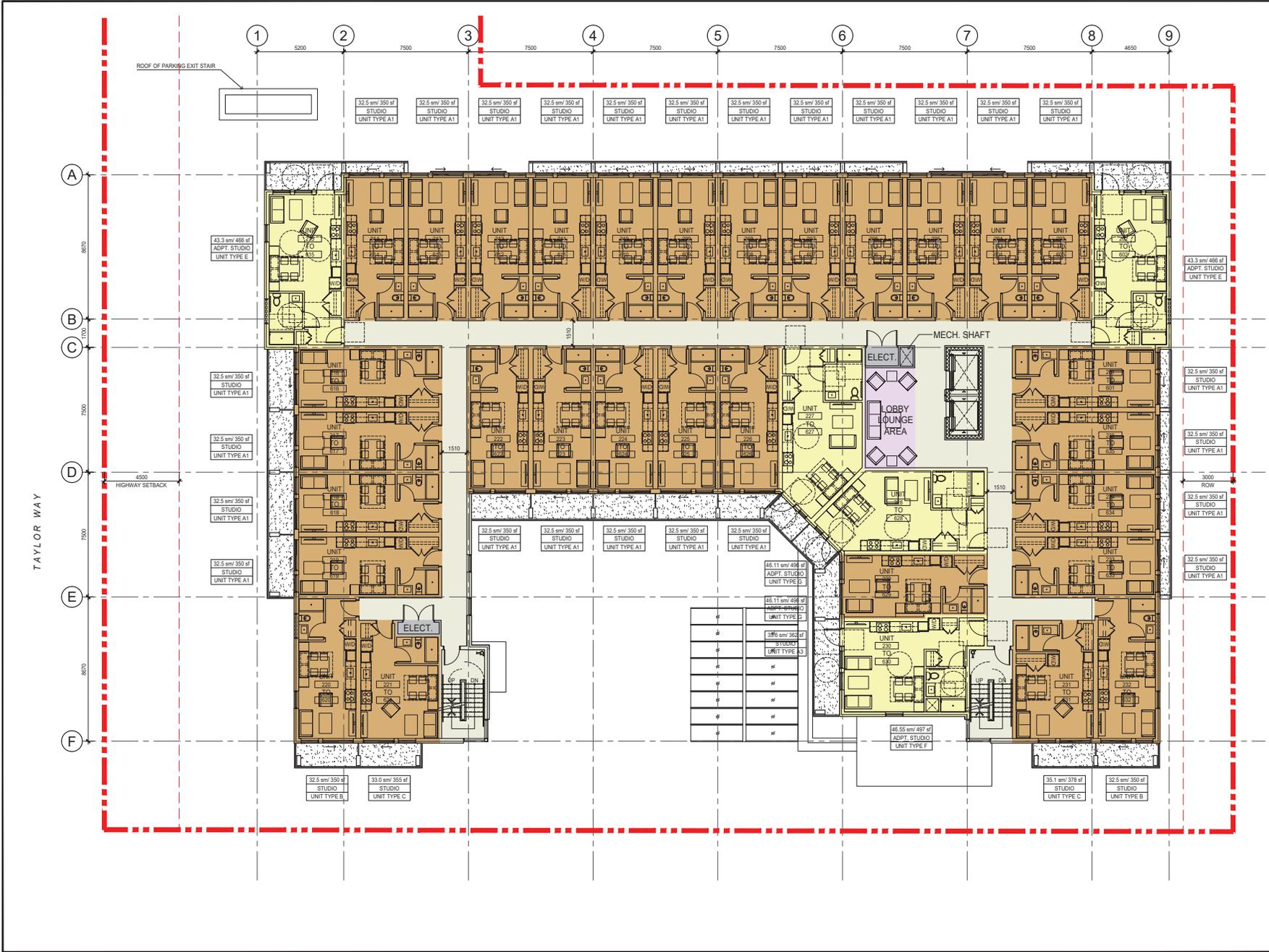
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Sheet Title  
**FLOOR PLAN  
 LEVEL 2-4**

Job No.	20-14-000	Sheet No.	
Scale	1:100	Checked	ME
Drawn	MS	Approved	ME
Checked	RK	Date	2021.11.24
Approved	ME	Revision	

A303











KEY PLAN



ISSUED FOR REZONING / DP	JAN 10 2022		
ISSUED FOR REZONING / DP	DEC 21 2021		
No.	Description	Date	Dr.
Revisors: Read Up			

1 NORTH ELEVATION  
1:100



MATERIALS LEGEND

- (A) BRICK CLADDING  
COLOUR: DARK GREY
- (B) CEMENTITIOUS PANEL CLADDING  
COLOUR: LIGHT BROWN
- (C) CEMENTITIOUS PANEL CLADDING  
C1 - COLOUR: GREY  
C2 - COLOUR: BLACK
- (D) PREFINISHED METAL PROFILE  
COLOUR: BLACK
- (E) WINDOW SYSTEM  
GLAZING: CLEAR LOW-E  
MULLIONS: BLACK
- (F) COLOURED (LAMINATED) GLASS  
COLOUR: SKY BLUE
- (G) PREFINISHED METAL RAILING SYSTEM  
G1 - COLOUR: LIGHT GREY
- (H) GLASS RAILING SYSTEM  
H1 - COLOUR: CLEAR GLASS  
H2 - COLOUR: CLEAR GLASS  
H3 - COLOUR: CLEAR GLASS
- (I) WOOD CANOPY STRUCTURE  
COLOUR: LIGHT BROWN
- (J) PAINTED STEEL STRUCTURE  
COLOUR: DARK BROWN
- (K) METAL RAILING SYSTEM  
COLOUR: BLACK

2 WEST ELEVATION  
1:100



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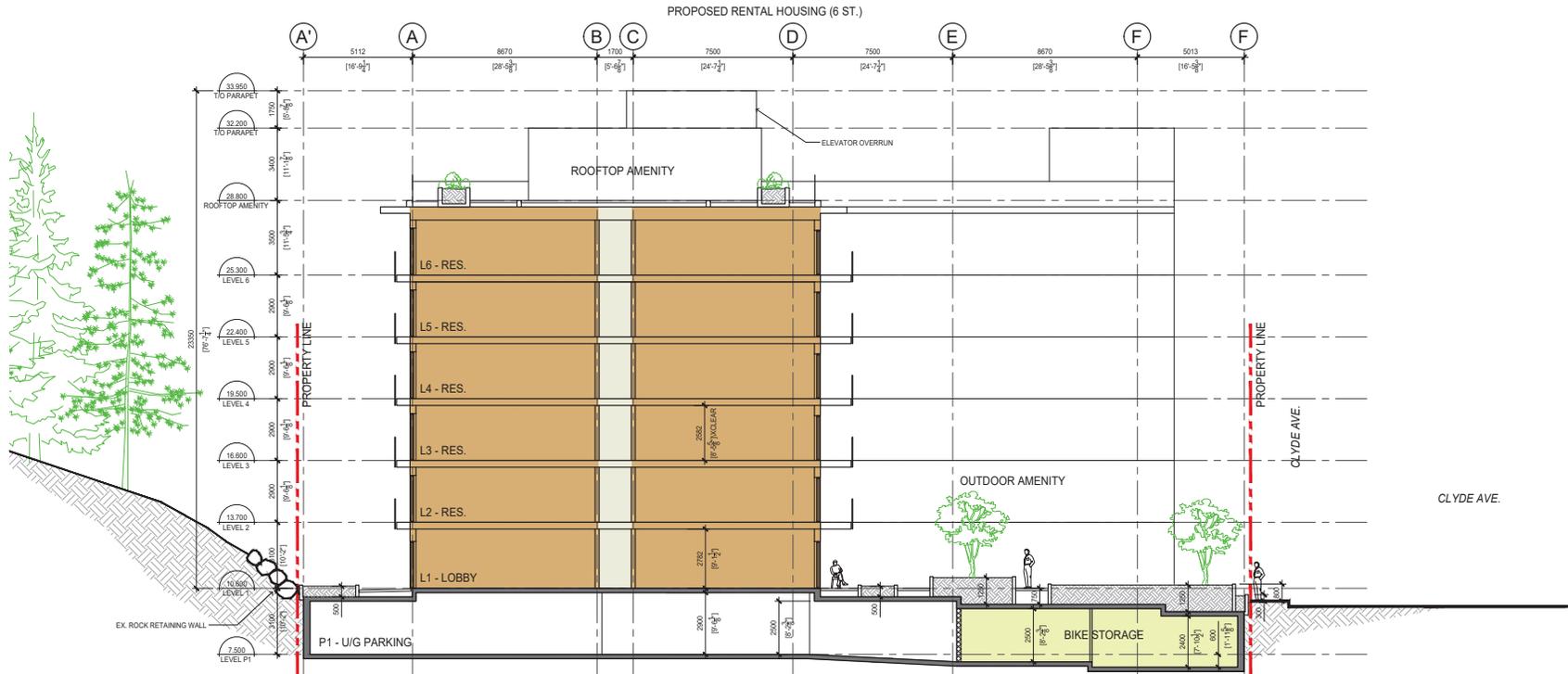
EXTERIOR ELEVATIONS

Job No.	20-14-000	Sheet No.	
Scale	1:200		
Drawn	MS		
Checked	RK		
Approved	ME		
Date	2021.11.24	Revision	

A321



KEY PLAN



ISSUED FOR REZONING / DP	JAN 10 2022		
ISSUED FOR REZONING / DP	DEC 21 2021		
No.	Description	Date	Dr.
Revisions: Read Up			

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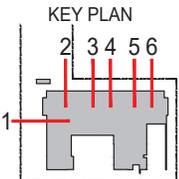
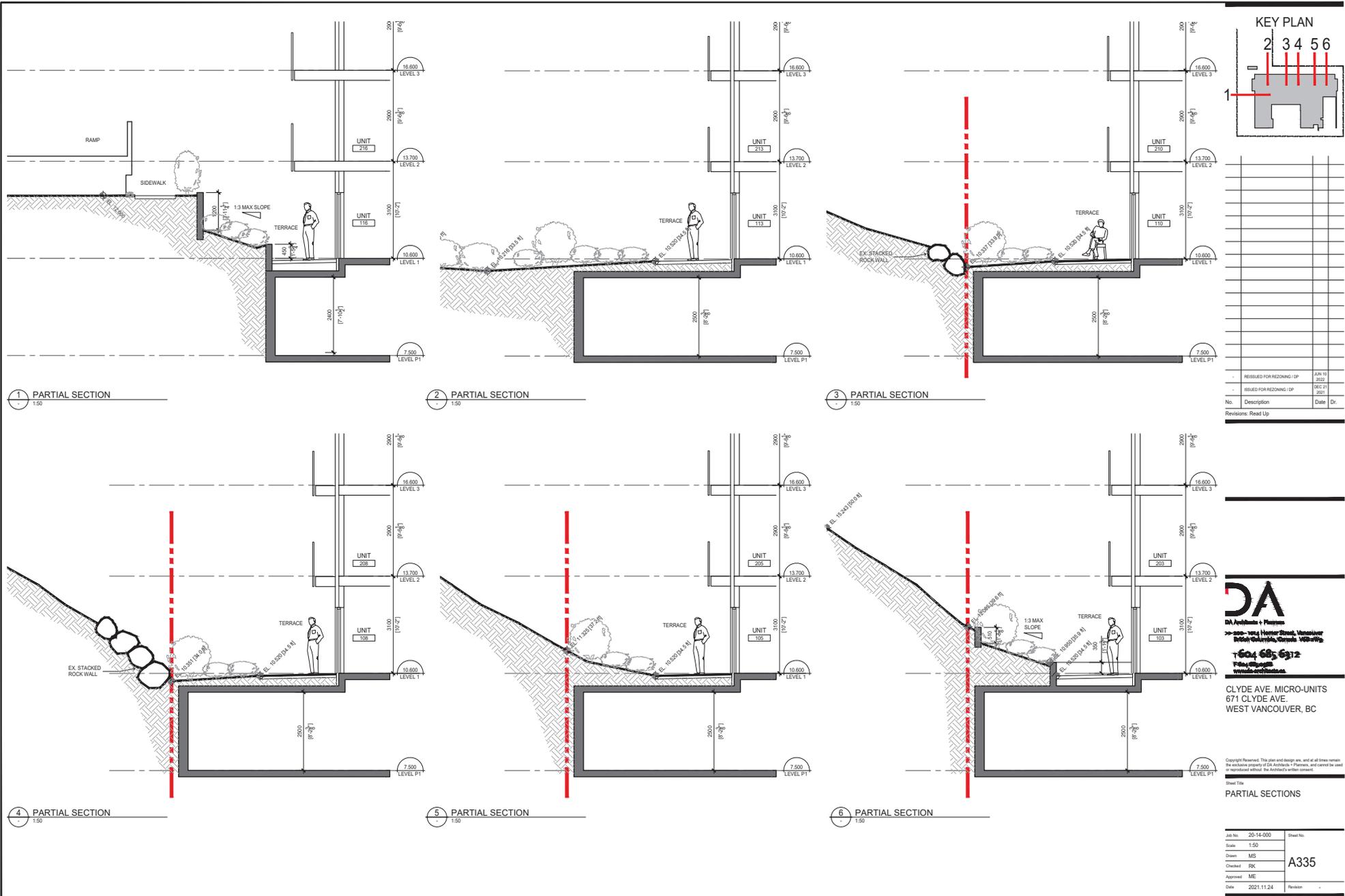
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Sheet Title  
**BUILDING SECTIONS  
NORTH-SOUTH**

Job No.	20-14-000	Sheet No.	
Scale	1:200		
Drawn	MS		
Checked	RK		<b>A330</b>
Approved	ME		
Date	2021.11.24	Revision	





No.	Description	Date	Dr.
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2	ISSUED FOR REZONING / DP	OCT 21 2021	

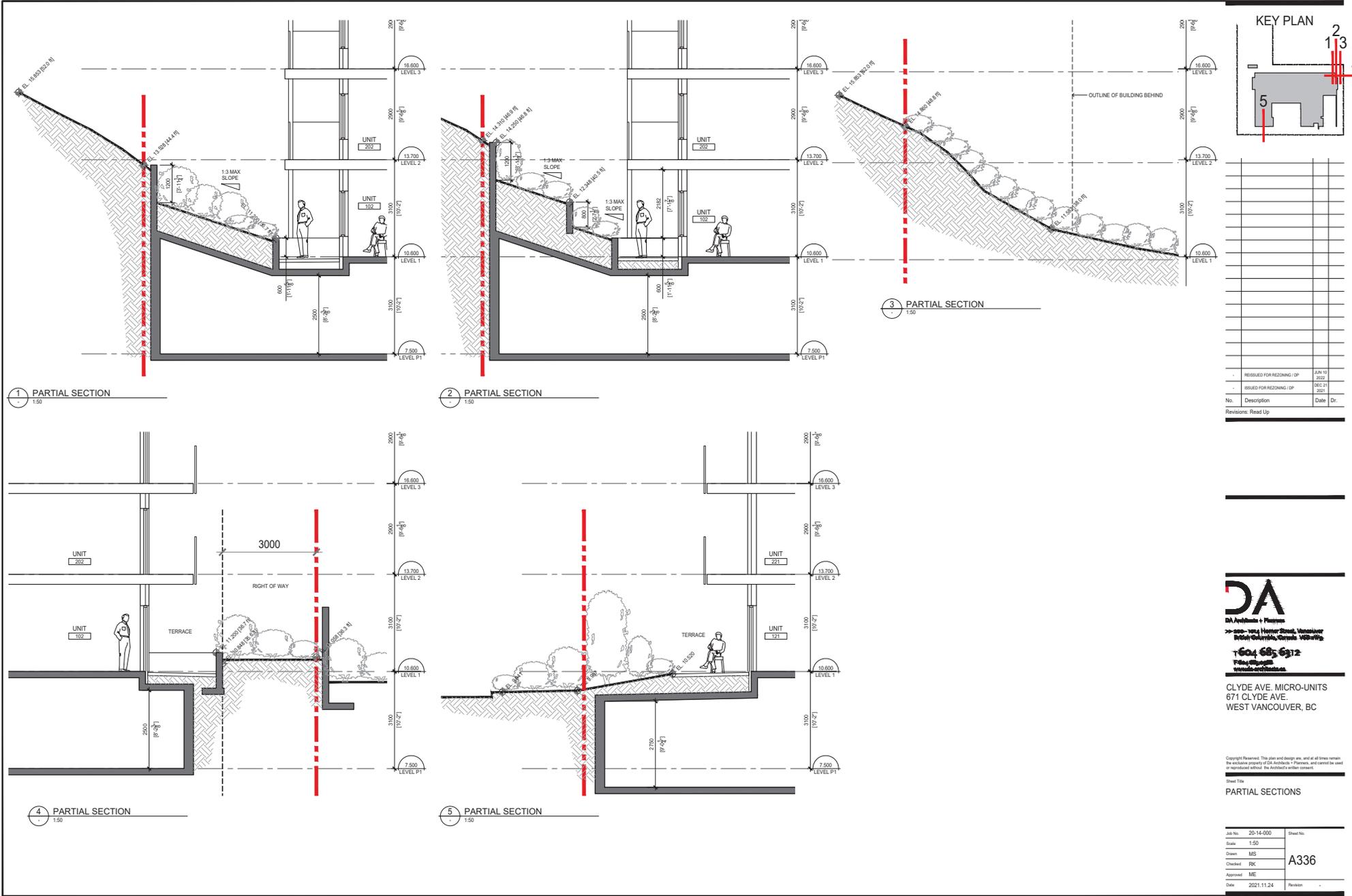
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Sheet Title  
**PARTIAL SECTIONS**

Job No.	20-14-000	Sheet No.	
Scale	1:50		
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Checked	RK		<b>A335</b>
Approved	ME		
Date	2021.11.24	Revision	





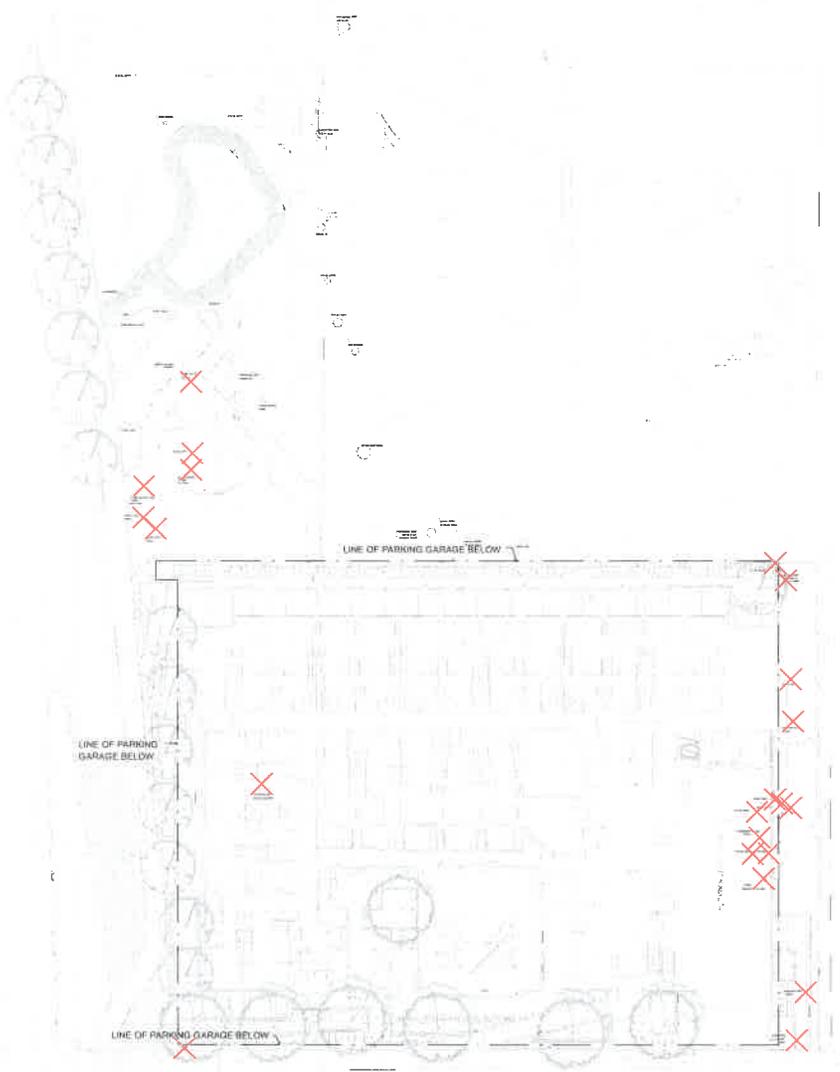












## TREE PROTECTION:

### 1 GENERAL

#### 1.1 RELATED WORK IN OTHER SECTIONS

Section 02210 - Site Grading  
 Section 02900 - Plants and Planting  
 Section 02921 - Topsoil and Finish Grading

#### 1.2 CONFORMANCE

1.2.1 Conform to Division 1 - General Requirements

#### 1.3 WORK INCLUDED

- 1.3.1 Furnish all labor, materials, equipment and services necessary to protect existing trees on site and on adjacent road right-of-way and sites, including but not limited to:
  - 1.3.2 Survey and layout for locations of protective barriers.
  - 1.3.3 Installation, maintenance, adjustment during construction, and final removal of protective barriers and signs.
  - 1.3.4 Pruning as required, including hand excavation and root pruning.
  - 1.3.5 Watering, fertilizing and all other measures directed by the Certified Arborist as required to maximize the health and prospects for survival of the trees.
  - 1.3.6 Provision of the services of a Certified Arborist to plan for and supervise tree protection measures.

### 1.4 GENERAL

- 1.4.1 The drip-line, i.e. the furthest horizontal extent of the branches, is considered the location of most important roots, however the root system actually extends far beyond that limit. Tree roots are very shallow, generally in the top 1' of soil. The key to tree retention is minimizing root loss and providing sites for decay. Once the limit of disruption is established, it must be demarcated on site and fenced off from all impacts of construction.
- 1.4.2 The certified arborist shall be notified (giving at least two days notice) of the timing for root pruning, paving, installation of protective barrier, and all other tree protection measures. The certified arborist shall do or supervise all root pruning, etc.
- 1.4.3 Excavation, soil stabilizing measures, shoring (if necessary) and related work shall be planned and executed such that no excavation or other construction activities occur within the Tree Protection Area.

### 1.5 QUALITY ASSURANCE

- 1.5.1 Inspection: The Contractor shall give at least forty-eight (48) hours notice to the Consultant and the Certified Arborist when the protective barrier locations are accurately located on site, before starting hand excavation and root pruning. Actions shall be taken when all work is being done along the line of the protective fence.

### 2 PRODUCTS

#### 2.1 PROTECTIVE BARRIER

2.1.1 Protective Barrier shall be a 4' high chain link fence (stainless steel link fence or temporary construction fencing, security installed, plastic, and securely fixed in the approved positions.

#### 2.2 TREE PROTECTION AREA SIGNS

2.2.1 Tree Protection Area signs shall be signs at least 800mm x 400mm, on perforated plywood or other acceptable weather resistant material, reading: TREE PROTECTION AREA. No Dumping. No Burning. No Storage. No Cutting. No Machinery.

#### 2.3 WATER, FERTILIZERS, MISCELLANEOUS

2.3.1 Water, fertilizers and miscellaneous materials shall be as specified in other sections of the specification and as directed by the Certified Arborist.

### 3 EXECUTION

#### 3.1 PROTECTIVE BARRIER

3.1.1 Before starting site work, install a clearly visible 1.2m high continuous chain link fence barrier or temporary fence at the approved lines for "Zones of No Disturbance". Maintain this barrier until Substantial Performance and remove from the site at that time. Support fence on steel posts driven vertically into the ground at 3m on center, or as other ways approved by the consultant.

#### 3.2 TREE PROTECTION AREA SIGNS

3.2.1 Install Tree Protection Area signs as specified on the snow fence barrier, total of five signs. Signs shall be well-secured and shall be maintained in place until Substantial Performance.

3.2.2 Take all measures necessary to prevent the following activities within tree protection areas except as authorized by the Consultant:

- β Storage of materials or equipment.
- β Blooming of soil or excavated materials.
- β Burning.
- β Excavation or trenching.
- β Cutting of roots or branches.

#### 3.3 ROOT PRUNING

3.3.1 Before the start of machine excavation, hand excavate along the established limit of excavation and prune all roots along the line. Cuts shall be clean, to avoid wounding the tree.

#### 3.4 WATERING AND FERTILIZING

3.4.1 Retained trees shall be watered thoroughly and deeply, as necessary to equipment needed to maintain plant health, without prolonged saturation of the root zone. The method, amount and frequency of watering shall be as recommended by the arborist.

3.4.2 Retained trees may require fertilizing to stimulate regrowth of lost roots and foliage. The fertilizer program shall be as recommended by the arborist.

#### 3.5 OTHER MEASURES

3.5.1 Other measures may be necessary for tree protection and ongoing survival, depending on site conditions. These may be determined during the initial planning for retention and excavation, or may be recommended by the arborist during the course of construction.

END OF SECTION 02115

No	Description	Date
2	REVISIONS FROM DP	08/10/22
1	REVISIONS FROM DP	03/21/22

Revisions: Read Up

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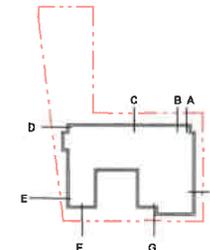
## TREE REMOVAL PLAN

Job No.	Sheet No.
1:200	
Drawn: MCV	
Checked: JMV	

L05  
r2



**SECTION INDEX**



No.	Description	Date
2	REVISIONS FROM OP	08/10/22
1	REVISIONS FROM CP	03/21/22

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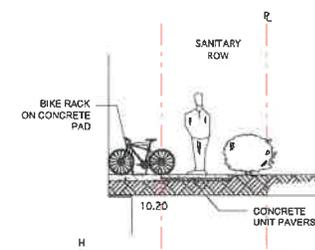
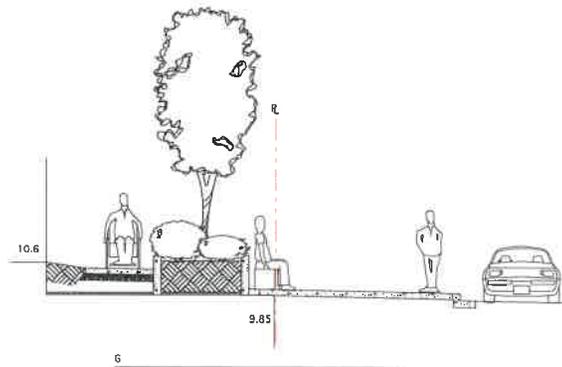
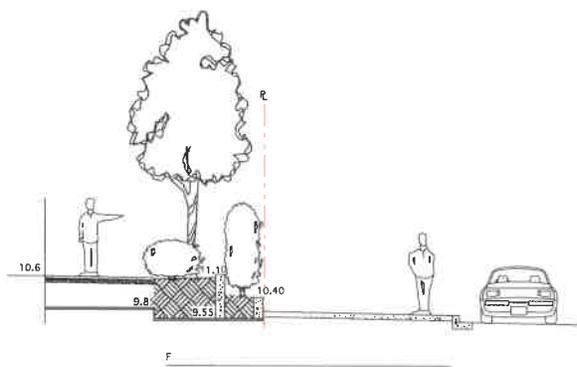
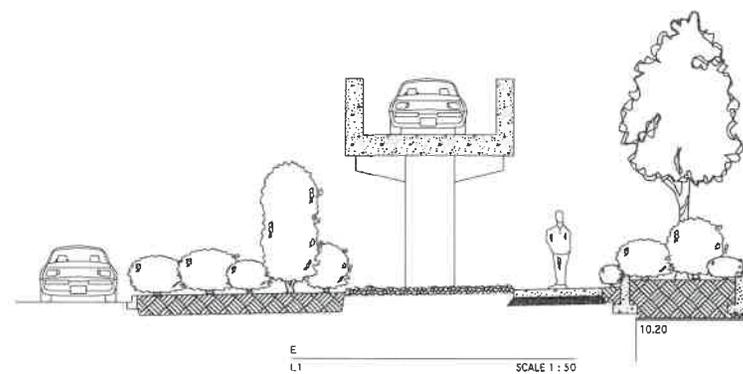
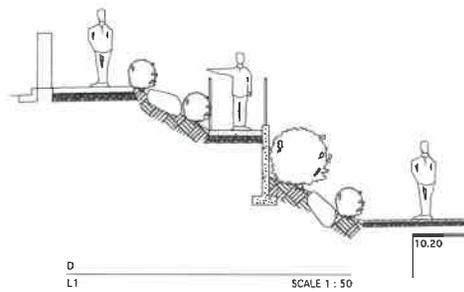
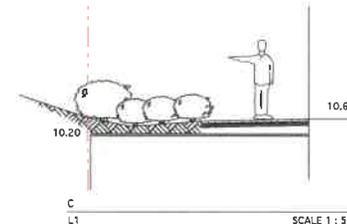
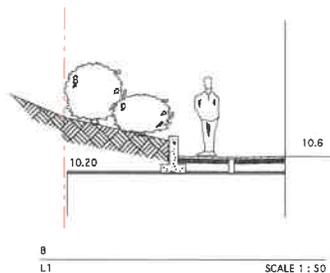
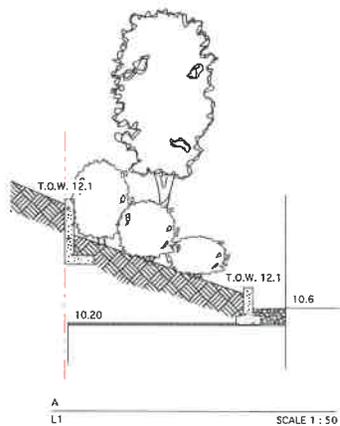
**Clyde Avenue Microunits**  
671, 685, 693 Clyde Ave  
& 694 Duchess Ave.

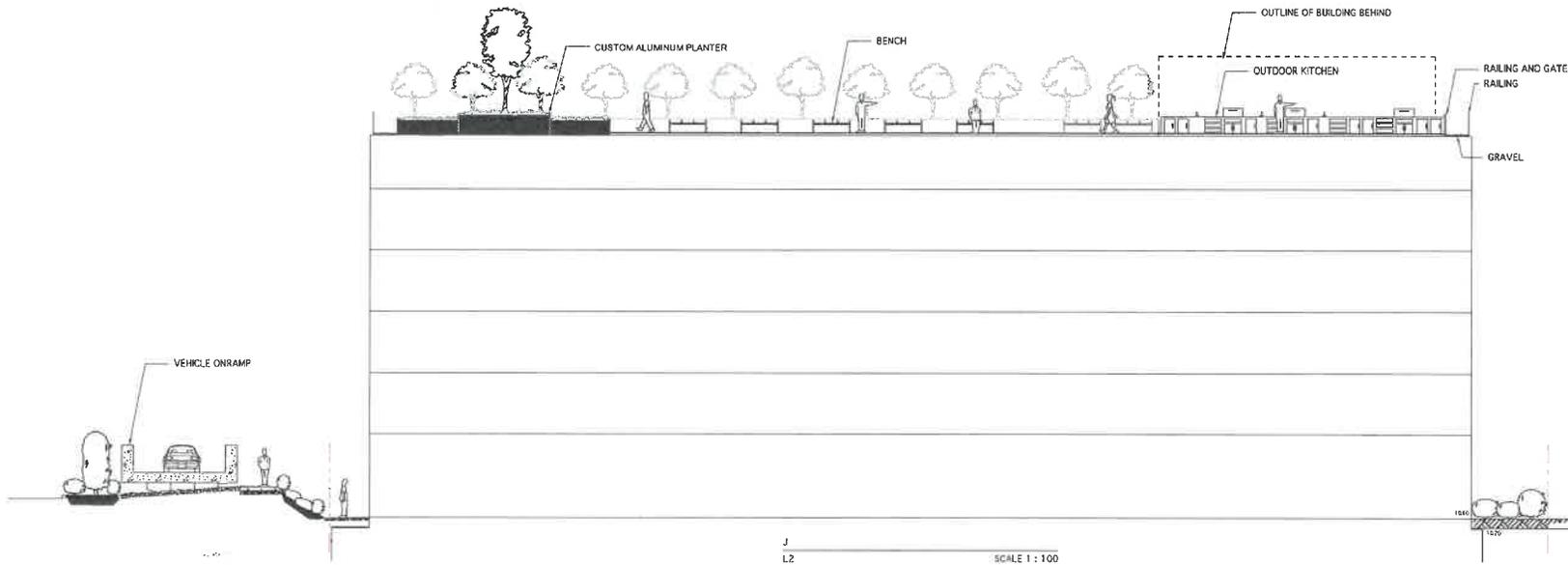
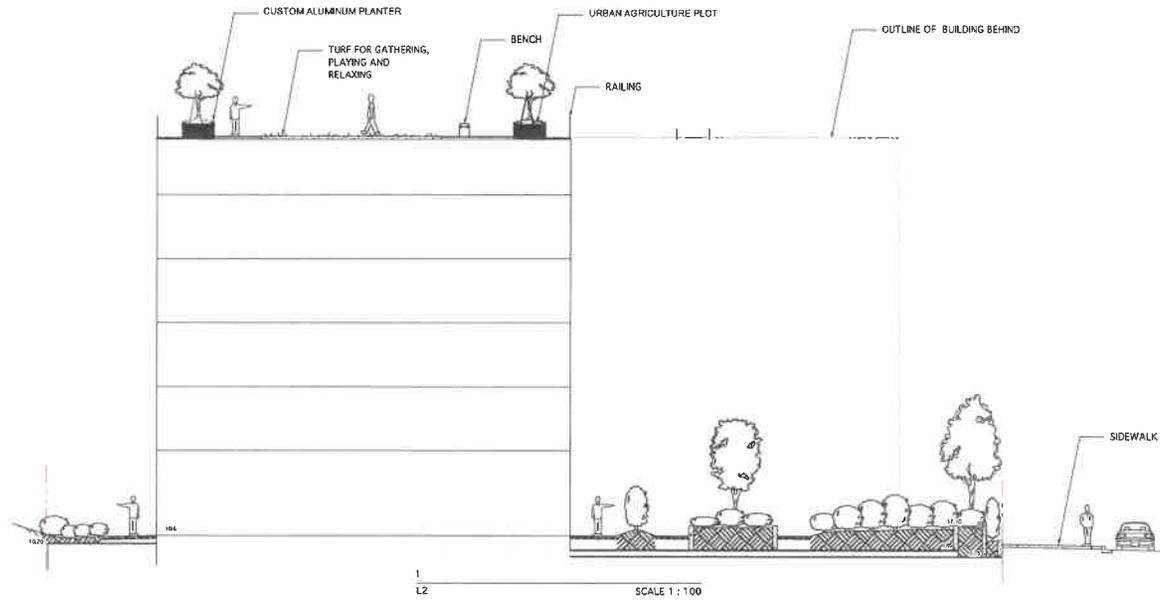
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**SECTIONS 1**

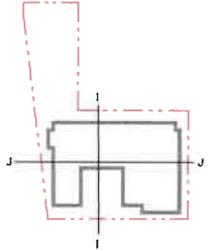
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SECTION INDEX



No	Description	Date
2	REVISIONS FROM DP	06/10/22
1	REVISIONS FROM DP	03/21/22

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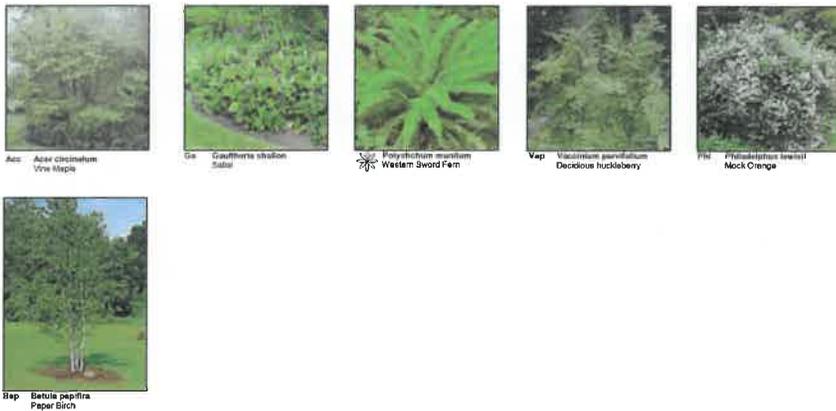


Plant List north and east of building

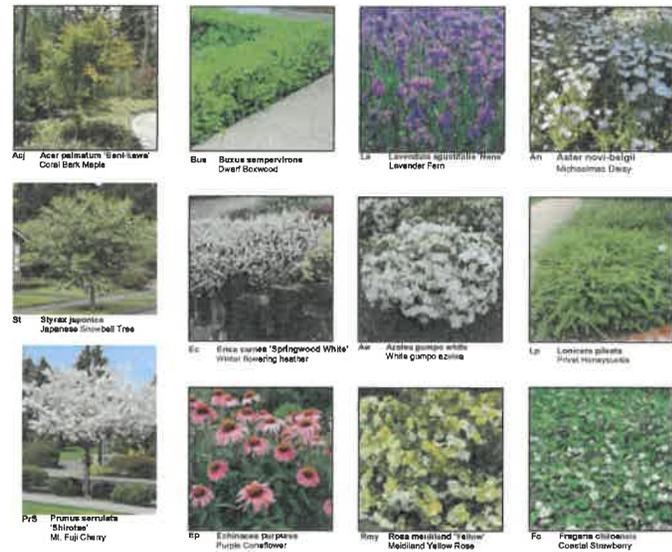
SYM	LATIN NAME	COMMON NAME	SIZE	CONDITION
<b>TREES</b>				
Acc	Acer circinatum	Vine Maple	3.5m. ht.	B&B
Bep	Betula papyrifera	Paper Birch	2.0 m. ht.	B&B
<b>SHRUBS</b>				
Ac	Achnatherum calamagrostis	Needle Grass	0.3m ht.	#3 pot
Bl	Blechnum spicant	Deer Fern		#1 pot
Cev	Ceanothus velutinus	Snowbush ceanothus	1.0 m. ht.	#3 Pot
Fc	Fragaria chiloensis	Coastal Strawberry		10 cm pot
Gs	Gaultheria shallon	Salal		10 cm. pot
Im	Iris missouriensis	Missouri Iris		#1 pot
Phl	Philadelphus lewisii	Lewis's Mock Orange	1.0 m. ht.	#5 pot
	Polystichum munitum	Western Sword Fern		#1 Pot
Ris	Ribes sanguineum	Red Flowering Currant	1.5 m. ht.	#5 pot
Sya	Symphocarpus albus	Common Snowberry	0.3m. ht.	#3 pot
Vap	Vaccinium parvifolium	Red Huckleberry	0.6 m. ht.	#3 Pot

All work to CSLA/CNTA Standards

SHRUBS



Plant List on Roof



Sym.	Latin Name	Common Name	Size	Cdn
<b>Trees</b>				
Accg	Acer palmatum 'sango-kaku'	Coral Bark Maple	1.5 m ht.	#10 pot
Prs	Prunus serrulata 'Mt. Fuji'	Mt. Fuji Japanese Flowering Cherry	7 cm. cal.	B&B
Styj	Styrax japonica	Japanese Snowbell Tree	2.5 m. ht.	B&B
<b>Shrubs</b>				
Aw	Azalea 'Gumpo White'	Gumpo White Azalea	0.3m ht.	#3 pot
Ec	Erica carnea 'springwood white'	Springwood White Flowering Heather	0.3m ht.	#3 pot
Lp	Lonicera pileata	Box-leaf Honeysuckle	0.3m ht.	#3 pot
Rmy	Rosa meidiland 'Yellow'	Meidiland Rose Yellow	0.3m ht.	#3 pot
<b>Groundcovers and Perennials</b>				
An	Aster novi-belgii	New York Aster		#1 pot
Ep	Echinacea purpurea	Purple Coneflower	0.3m ht.	#3 pot
Fo	Festuca ovina 'Glaucia'	Elijah Blue Fescue		#1 pot
Fc	Fragaria chiloensis	Coastal Strawberry		10 cm pot
La	Lavandula angustifolia 'Nana'	Dwarf Lavender	0.3m ht.	#3 pot
Ps	Phlox subulata 'Snowflake'	Creeping Phlox		10 cm pot
Rh	Rudbeckia hirta 'Goldstrum'	Black Eyed Susan	0.3m ht.	#3 pot

No	Description	Date
2	REVISIONS FROM OP	05/10/22
1	REVISIONS FROM OP	03/21/22

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PLANT LIST  
ROOF AND  
NORTH

Job No. \_\_\_\_\_ Sheet No. \_\_\_\_\_  
Scale \_\_\_\_\_  
Drawn: MCV  
Checked: JMV

L11 r2

TREES



Quc *Quercus coccinea*  
Scarlet Oak



Acr *Acer rubrum* 'Franks Red'  
Red Maple Franks Red



Acp *Acer palmatum* 'Semi-Sarai'  
Coral Bark Maple



Stj *Styrax japonica*  
Japanese Snowbell Tree



Prs *Prunus serrulata* 'Shirotae'  
Mt. Fuji Cherry

SHRUBS



Bux *Buxus sempervirens*  
Dwarf Boxwood



Ec *Erica carnea* 'Springwood White'  
Winter Flowering Heather



Ep *Echinacea purpurea*  
Purple Coneflower



Cae *Carex* 'Feather Reed Grass'  
Feather Reed Grass



Fc *Fragaria chiloensis*  
Coastal Strawberry



Az *Azalea japonica* 'White'  
White Gumpo Azalea



Rmy *Rosa meidland* 'Yellow'  
Meidland Yellow Rose



Fg *Festuca gauderi*  
Deerkill Fescue



Sr *Sarcococca nana* 'Fragrant Sweet'  
Fragrant Sweet



Ch *Choisya ternata*  
Mexican orange



Rm *Rosa meidland* 'White'  
Meidland White Rose



Ht *Hosta* 'various'  
Hosta - variety of selections



Ga *Gaultheria shallon*  
Salal



An *Aster novi-belgii*  
Michaelmas Daisy



Lp *Lonicera pileata*  
Privet Honeysuckle



La *Lavandula angustifolia* 'Nana'  
Lavender Foli



Az *Azalea japonica* 'White'  
White Gumpo Azalea



Pol *Polystichum munitum*  
Western Sword Fern



Cal *Ceanothus impressus* 'Victoria'  
California Lilac



Rh *Rhododendron* 'Snow Lady'  
Snow Lady's Rhododendron

Plant List area around building

Sym.	Latin Name	Common Name	Size	Cdn
<b>Trees</b>				
Acp	<i>Acer palmatum</i> 'sango-kaku'	Coral Bark Maple	1.5 m ht.	#10 pot
Acr	<i>Acer rubrum</i> 'Franks Red'	Franks Red' Red Maple	7 cm. cal.	Cage
Prs	<i>Prunus serrulata</i> 'Kwanzan'	Kwanzan Japanese Flowering Cherry	6 cm. cal.	B&B
Prs	<i>Prunus serrulata</i> 'Mt. Fuji'	Mt. Fuji Japanese Flowering Cherry	7 cm. cal.	B&B
Quc	<i>Quercus coccinea</i>	Scarlet Oak	7 cm. cal.	Cage
Stj	<i>Styrax japonica</i>	Japanese Snowbell Tree	2.5 m. ht.	B&B
<b>Shrubs</b>				
Aw	<i>Azalea</i> 'Gumpo White'	Gumpo White Azalea	0.3m ht.	#3 pot
Cev	<i>Ceanothus velutinus</i>	Snowbush ceanothus	1.0m ht.	#3 pot
Ch	<i>Choisya ternata</i>	Mexican Orange	0.5m ht.	#3 pot
Ec	<i>Erica carnea</i> 'springwood white'	Springwood White Flowering Heather	0.3m ht.	#3 pot
Hs	<i>Hosta seiboldiana</i>	Hosta	0.2m ht.	#1 pot
Lp	<i>Lonicera pileata</i>	Box-leaf Honeysuckle	0.3m ht.	#3 pot
	<i>Polystichum munitum</i>	Western Sword Fern		#1 pot
Rmy	<i>Rosa meidland</i> Yellow	Meidland Rose Yellow	0.3m ht.	#3 pot
<b>Groundcovers and Perennials</b>				
An	<i>Aster novi-belgii</i>	New York Aster		#1 pot
Ep	<i>Echinacea purpurea</i>	Purple Coneflower	0.3m ht.	#3 pot
Fo	<i>Festuca ovina</i> "Glaucua"	Elijah Blue Fescue		#1 pot
Fc	<i>Fragaria chiloensis</i>	Coastal Strawberry		10 cm pot
Gs	<i>Gaultheria shallon</i>	Salal		10 cm pot
La	<i>Lavandula angustifolia</i> 'Nana'	Dwarf Lavender	0.3m ht.	#3 pot
Ps	<i>Phlox subulata</i> "Snowflake"	Creeping Phlox		10 cm pot
Rh	<i>Rudbeckia hirta</i> 'Goldstrum'	Black Eyed Susan	0.3m ht.	#3 pot

No	Description	Date
2	REVISIONS FROM DP	06/10/22
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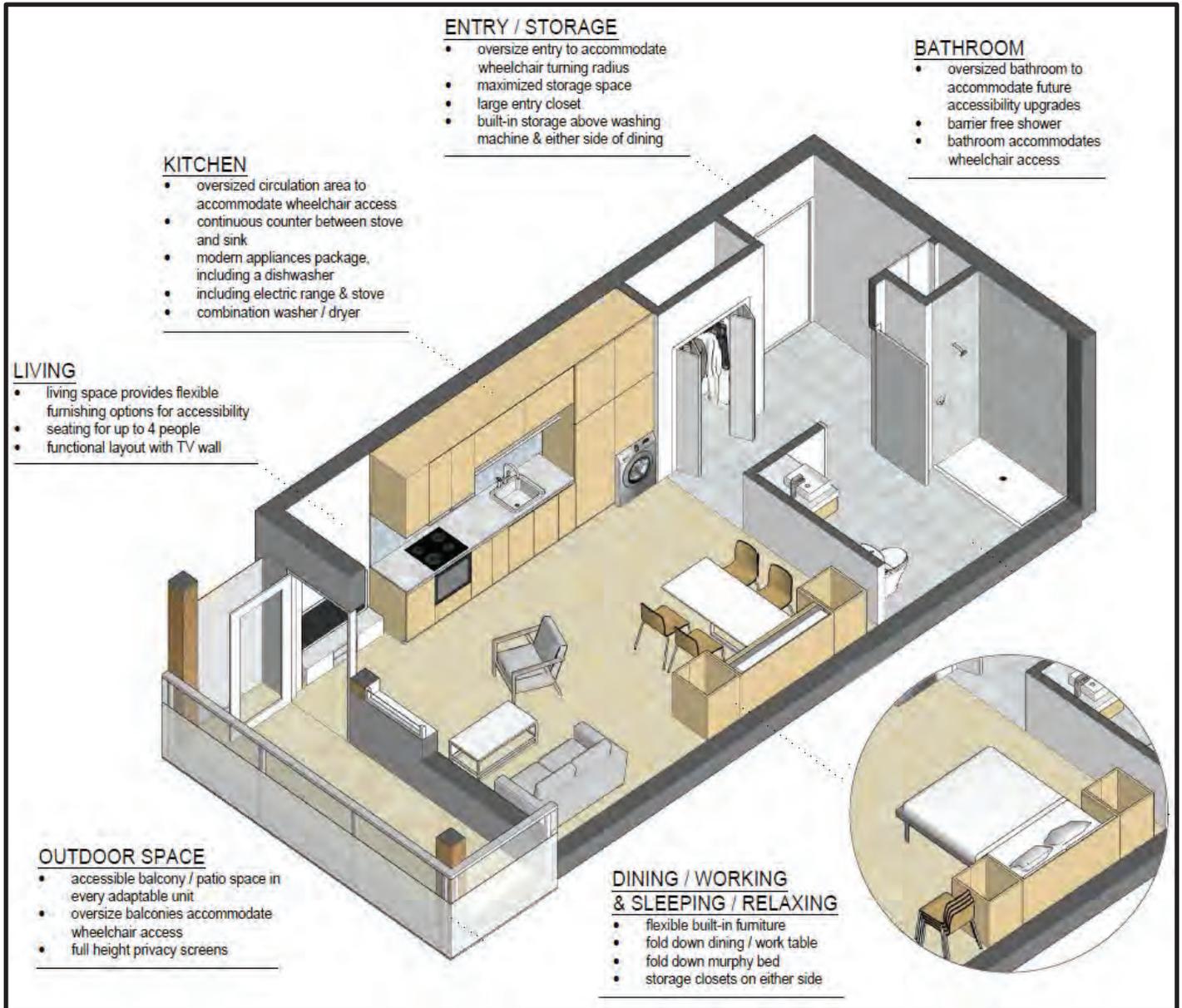
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PLANT LIST  
AROUND  
BUILDING

Job No.	Sheet
Scale	No.
Drawn	MCV
Checked	JNV

L10 r2

## Appendix F – Typical Layout of Accessible Units



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# Clyde Avenue Micro Units

(671, 685, 693 Clyde Avenue, 694 Duchess Avenue)

In Person and Virtual Developer Public  
Information Meetings and Developer Open House  
Summary Report

*Prepared by Park Royal Shopping Centre Holdings Ltd.  
December 15, 2021*



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PARK ROYAL

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# OVERVIEW

Further to the Preliminary Development Proposal for 671, 685, 693 Clyde Avenue and 694 Duchess Avenue submitted to the District of West Vancouver (DWV) on July 21, 2021 and the October 4, 2021 West Vancouver Council Council's motion stating "THAT the preliminary development proposal outlined within the report titled Preliminary Development Proposal for 671, 685, 693 Clyde Avenue and 694 Duchess Avenue dated September 15, 2021 from the Senior Community Planner proceed to public consultation prior to adoption of the Taylor Way Local Area Plan in accordance with Council's Preliminary Development Proposal and Public Consultation Policy.", Park Royal Shopping Centre Holdings Ltd. (Park Royal) has conducted the following public consultation.

November 24, 2021, Park Royal hosted an In-Person Public Information Meeting on the development proposal and in compliance with the DWV Public Consultation Policy. The Public Information Meeting was at 779 Main Street in Park Royal South, from 6:00pm to 8:00pm in West Vancouver, providing the opportunity for the public to meet the project team, learn about the proposal, ask questions and provide feedback.

Park Royal hosted a Virtual Public Information Meeting on the development proposal in compliance with the DWV Public Consultation Policy. The Virtual Meeting opened 9am on Monday November 22nd and closed at 5pm on Monday November 29th, 2021. This was accessed at: [clydemicrounits.com/feedback](https://clydemicrounits.com/feedback)

The Virtual Meeting included application information, FAQs and the ability to email questions and comments to the Park Royal consultant team.

Additionally, Park Royal hosted a Public Open House at the same location as the Public Information Meeting from November 19-28, 2021 to further inform the public, ask questions, and provide feedback about the proposal. All visitors to the Open House were encouraged to attend either the Virtual or Physical Public Information Meeting.

## **PUBLIC CONSULTATION SUMMARY**

### **In-Person Public Information Meeting Outcome--November 24, 2021,**

- Thirty members of the public attended, including one District of West Vancouver staff member
- All members of the public were encouraged to fill out a comment form that night. Those who did not, were encouraged to scan a QR code which took them to the virtual meeting, where a comment form was available to fill out. Seven comment forms were completed and returned to Park Royal that night

Of the seven comments received:

- 100% were supportive or supportive with additional suggestions
- 67% of the Comment Forms submitted were from residents of West Vancouver
- 0% were non-supportive

### **Virtual Public Information Meeting Outcome-- November 22-29, 2021**

- One hundred fifty-nine people visited the on-line Virtual Public Information Meeting Site
- Twenty-nine comment forms were completed

Of the twenty-nine comments received:

- 97% were supportive or supportive with additional suggestions
- 3% (1) expressed concern based on the incorrect assumption that the property belonged to the District of West Vancouver
- 0% were non-supportive
- 67% of the Comment Forms submitted were from residents of West Vancouver

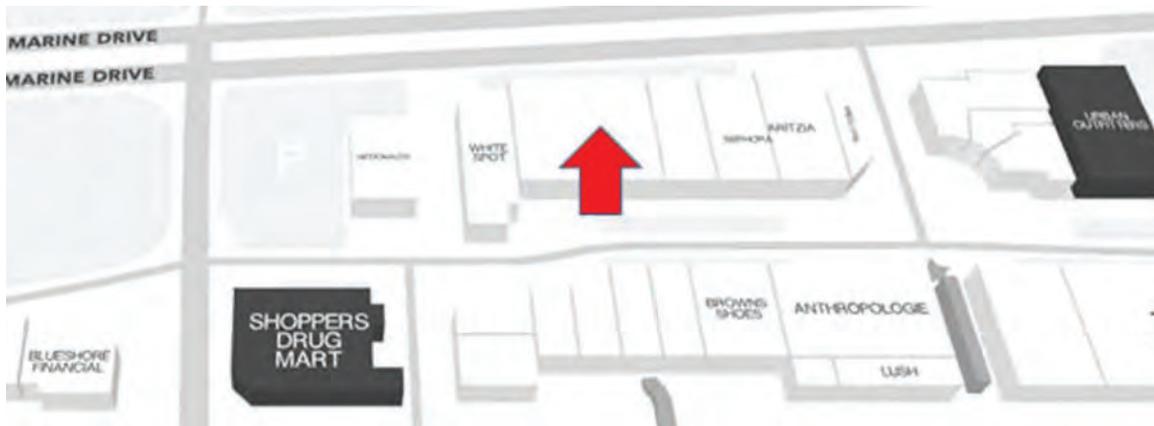
# 1.0 INTRODUCTION

Park Royal Shopping Centre Holdings Ltd. has applied to the District of West Vancouver for a rezoning and development permit for 671, 685, 693 Clyde Avenue and 694 Duchess Avenue.

The project team hosted *both* a Virtual and In Person Public Information Meeting to share information about the proposal with members of the public and gather feedback.

**PHYSICAL MEETING:** November 24, 2021

**VIRTUAL MEETING:** November 22 – 29, 2021, 779 Main St, Park Royal South



**Location of In-Person Public Information Meeting**

## 2.0 NOTIFICATION DETAILS

While residents and businesses within a 100-metre radius received notices according to District of West Vancouver requirements, in order to garner feedback from a greater representation of residents and businesses--appendix B (approximately 701 mailing addresses). All were notified about the Public Information Meeting via Canada Post addressed mail. The community was also notified through newspaper advertisements. Additionally, information about the Virtual and In -person Public Information Meetings and the 10-day Open House was featured on the Park Royal website (40,000 users per month), which was bolstered by Twitter (2,462 followers) and Facebook (11,215 Facebook friends)

### **Mail Notification**

701 Public Information Meeting notification flyers were distributed through mail by Canada Post on November 12, 2021.

See Appendix A for a copy of the notification flyer.

See Appendix B for the notification area.

### **Newspaper Advertisement**

A newspaper advertisement in the North Shore News ran on November 10 and 17, 2021.

See Appendix C for a copy of the newspaper advertisement.

# 3.0 MEETING FORMAT

## Event Details

**PHYSICAL MEETING:** 779 Main Street, Park Royal South  
**DATE:** November 24, 2021  
**TIME:** 6:00pm – 8:00pm

The Physical Meeting was a drop-in open house. Social distancing, sanitization and personal protection measures were in place as per WorkSafe BC requirements.

**VIRTUAL MEETING:** [clydemicrounit.com/feedback](https://clydemicrounit.com/feedback)  
**DATE:** November 22-29, 2021

The Virtual Meeting opened 9am on Tuesday November 23rd and closed at 5pm, Thursday November 25th, 2021. It was accessed at: [clydemicrounit.com/feedback](https://clydemicrounit.com/feedback)

The Virtual Meeting included application information, FAQs and the ability to email questions and comments the Park Royal consultant team.

## In-Person Event Description

The in-person Public Information Meeting took place on Wednesday November 24th from 6:00 pm to 8:00 pm in Park Royal South, next door to the White Spot. Upon arrival, attendees were invited to sign-in, review the display boards, ask questions, and fill out a comment form. The display boards were arranged along the south, east and north walls. A diagram, depicting the size of a typical micro unit and typical adaptable micro unit was taped to the floor. Tables were placed near the exit of the room for attendees to fill out comment forms before leaving. Members of the project team were available to answer questions throughout the event.

## Project Team in Attendance

### *Park Royal*

- Rick Amantea
- Wendy LeBreton
- Misti Mussatto

### *DA Architects*

- Ross Komnatskyy
- Mark Ehman

### *Bunt & Associates Engineering Ltd.*

- Peter Joyce

### *Vaughan Landscape*

- Mark Vaughan

## 4.0 PRESENTATION MATERIAL

The In-Person Public Information Meeting board titles, which are representative of the content, are listed below.

1. Directional Welcome
2. Welcome
3. Purpose of this Public Information Meeting
4. Executive Summary
5. Proposal Existing & Future Context/Project Description/Project Design Rationale/Compliance with OCP policies
6. Existing Context: Neighbourhood
7. Existing Context: Development
8. Existing Context: Photos
9. Site Plan
10. Site Sections
11. Floor Plans
12. Micro Unit Concept—Typical Micro Unit
13. Micro Unit Concept—Typical Adaptable Micro Unit
14. Target Market: Workplace & Young Professionals Housing
15. 3D Views: North & South West Corners—existing and future context
16. 3D Views: North, South, East, West elevation
17. 3D Views: Aerial
18. Sustainability
19. Transportation – Options
20. Transportation – Traffic
21. Landscape (2 slides)

See Appendix E for a copy of the boards.

## 5.0 COMMENT FORM SUMMARY

Comment forms were available to attendees when they entered and exited for both the Public Open House and In Person Public Information Meeting. Everyone was encouraged to complete one, and those who didn't were encouraged to participate in the Virtual Public Information Meeting.

A total of 9 comment forms were completed in person from both in person events. **(67% of respondents were West Vancouver residents)**

A total of 29 comment forms were completed virtually **(67% of respondents were from West Vancouver)**

### *Comment Form Responses*

There were no specific questions asked on the in-person comment forms.

Of the 9 comment forms completed and returned:

- 4 mentioned affordability, 44%
- 5 mentioned employment housing, 56%
- 1 mentioned replacing an "eyesore" 11%

See Appendix E for a copy of the comment form.

See Appendix F for In Person comment forms completed transcriptions and Appendix G for Virtual comments

# 6.0 APPENDICES

## Appendix A: Notification Flyer - *Front*

### PLEASE JOIN US

#### Applicant Hosted Preliminary Public Information Meeting

Proposed Rezoning of 671, 685, 693 Clyde Avenue and 694 Duchess Avenue

**MEETING LOCATION:** 779 Main Street, Park Royal South (next to White Spot)  
**DATE:** November 24, 2021  
**TIME:** 6:00pm – 8:00pm  
**APPLICANT:** Park Royal Shopping Centre Holdings Ltd.



Rendering of Proposal (looking eastward)



Location Map/Site Plan

#### The Proposal

Park Royal Shopping Centre Holdings Ltd. plans to submit a rezoning and development permit application for residential rental units at 671, 685 and 693 Clyde Avenue that includes:

- 201 residential “micro-unit” rentals (2.0 FAR)
- Approximately 49 parking stalls (.25 stalls per unit)
- Approximately 302 bicycle stalls (1.5 per unit)
- 6 storey residential building with landscaped rooftop amenity
- Northern lot (694 Duchess Avenue) outdoor green space or other amenity

#### Online Preliminary Public Information Meeting

If you prefer to provide your comments online (rather than at the in-person meeting), please visit: [www.clydemicrounits.com/feedback](http://www.clydemicrounits.com/feedback). The webpage will be open for comments from November 22, 2021 to November 29, 2021.

#### Questions or Comments?

Please contact Misti Mussatto [mmussatto@parkroyal.ca](mailto:mmussatto@parkroyal.ca) or at (604) 923-4728.

#### Need More Information?

Please visit the District of West Vancouver’s preliminary development application overview web page at <https://westvancouver.ca/micro>

## Appendix A: Notification Flyer - Back



Aerial View—Looking Northeast

### Meeting Details

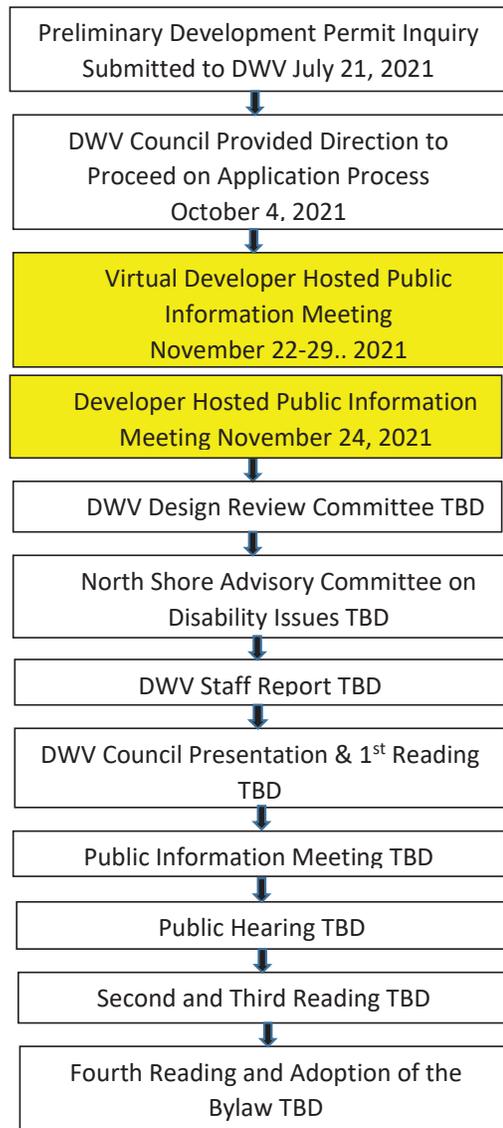
Park Royal Shopping Centre Holdings Ltd. is hosting a Public Information Meeting on November 24, 2021 between 6:00pm - 8:00pm at Park Royal South, 779 Main Street.

The purpose of this event is to share information on the proposal and receive public feedback.

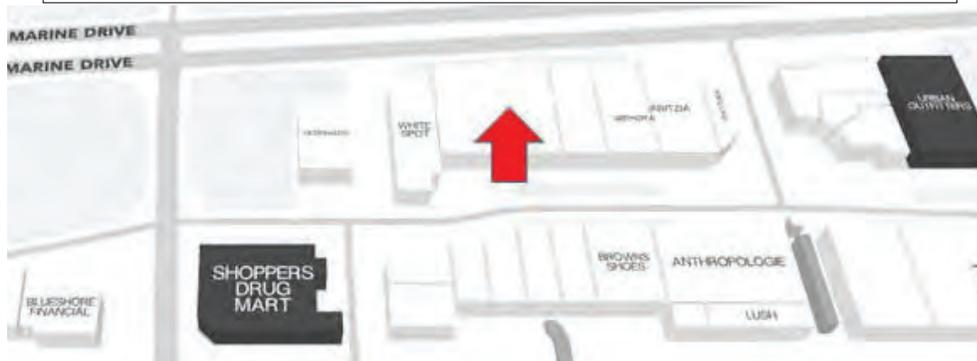
The meeting format is a drop-in open house. Members of the public are invited to attend, view display material, ask questions, and provide comments. There will be no formal presentation. The meeting is hosted by Park Royal Shopping Centre Holdings Ltd. and is not a District of West Vancouver event.

The In-Person Public Information Meeting location will be sanitized and all Worksafe BC protocols will be in place.

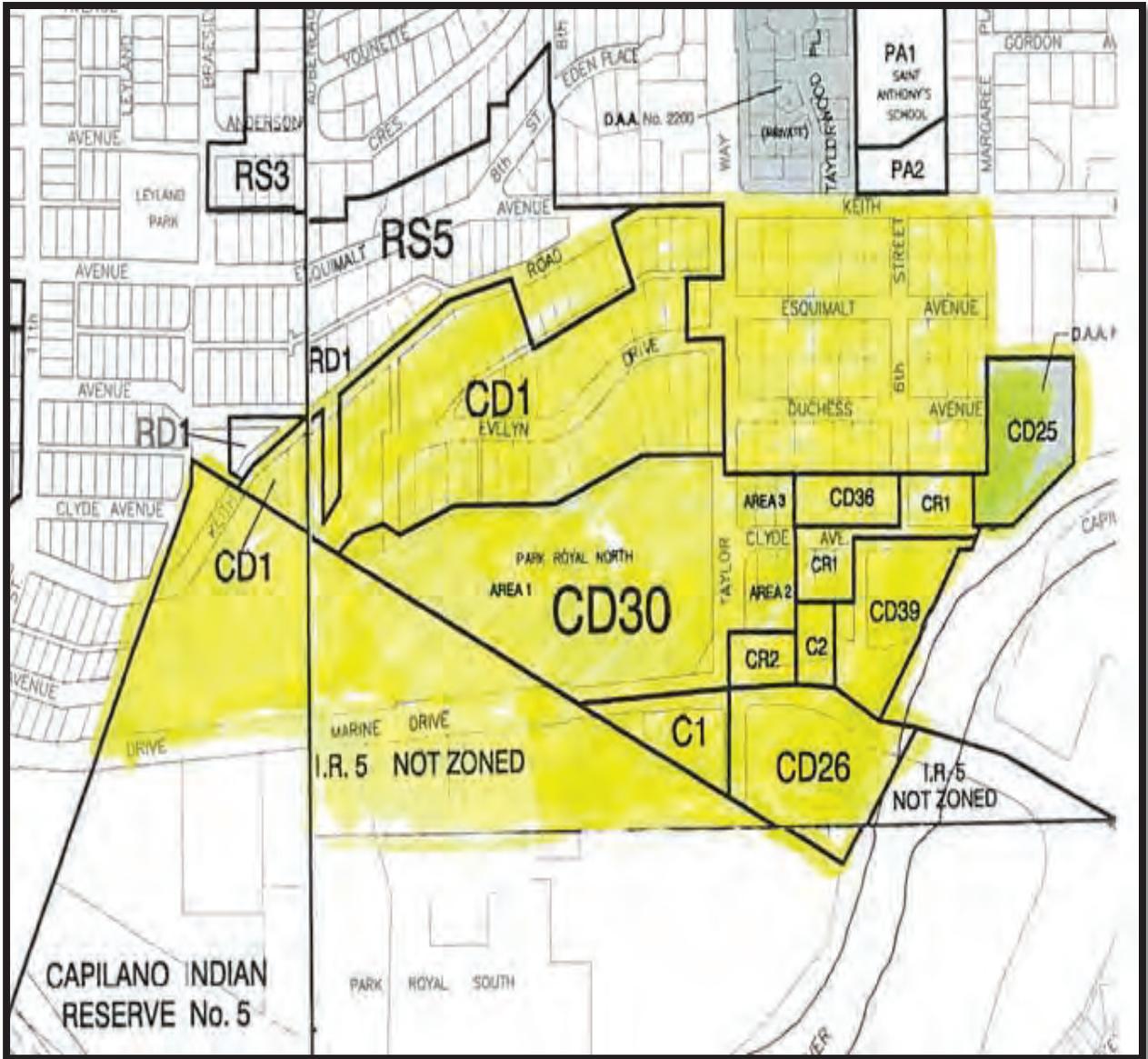
### Process



**MEETING LOCATION: 779 MAIN STREET PARK ROYAL SOUTH, NEXT TO WHITE SPOT**



# Appendix B: Notification Area



## Appendix C: Newspaper Advertisement

### PLEASE JOIN US

#### Applicant Hosted Preliminary Public Information Meeting

**Proposed Rezoning of 671, 685, 693 Clyde Avenue and 694 Duchess Avenue**

**MEETING LOCATION:** 779 Main Street, Park Royal South (next to White Spot)  
**DATE:** November 24, 2021  
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**APPLICANT:** Park Royal Shopping Centre Holdings Ltd.



Rendering of Proposal (looking eastward)



Location Map/Site Plan

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- 6 storey residential building with landscaped rooftop amenity
- Northern lot (694 Duchess Avenue) outdoor green space or other amenity

#### Online Preliminary Public Information Meeting

If you prefer to provide your comments online (rather than at the in-person meeting), please visit: [www.clydemicrounits.com/feedback](http://www.clydemicrounits.com/feedback). The webpage will be open for comments from November 22, 2021 to November 29, 2021.

#### Questions or Comments?

Please contact Misti Mussatto [mmussatto@parkroyal.ca](mailto:mmussatto@parkroyal.ca) or at (604) 923-4728.

#### Need More Information?

Please visit the District of West Vancouver’s preliminary development application overview web page at <https://westvancouver.ca/micro>

## Appendix D: Display Boards

Welcome to  
**Clyde Ave. Microunits**  
671, 685, 693 Clyde Ave.  
& 694 Duchess Ave., West Vancouver



### **PUBLIC INFORMATION MEETING**

Date: November 24, 2021  
Hosted by:  
**PARK ROYAL**



Welcome to  
**Clyde Ave. Microunits**  
671, 685, 693 Clyde Ave  
& 694 Duchess Ave., West Vancouver

**PUBLIC  
INFORMATION  
MEETING**

Date: November 24, 2021  
Hosted by:  
**PARK ROYAL**



# PURPOSE OF THIS PUBLIC INFORMATION MEETING

## GOALS:

- To present the Clyde Ave. Micro Units proposal to the public
- To understand the opportunities and benefits of this proposal
- To receive public comments and feedback

## STATISTICAL SUMMARY:

Current zoning: UH 18, 19, 40  
Site ID: 201

	CMR	HA	Proposed (cmr)	5.7	11
Total Project Site Area:	4,095.50	65,624			
Minimum FSR Area:	0.00		Site Yield Setback:	3.3	11
Proposed FSR Area:	3,113.00	67,838	Site Yield Setback:	5.3	11
Proposed FSR Area:	0.00				
Proposed Height:	6 stories				
Proposed Units:	301				

	Single Room Area	TM Exclusion	TM Area	Units
Level #1	2,430.00	0.00	2,430.00	26,010
Level #2	1,675.00	4,477.00	2,802.00	2,802
Level #3	1,400.17	16,481.00	36.00	281
Level #4	1,400.17	16,481.00	36.00	281
Level #5	1,400.17	16,481.00	36.00	281
Level #6	1,400.17	16,481.00	36.00	281
Level #7 - Rooftop Terrace	1,400.17	16,481.00	36.00	281
Level #8 - Rooftop Terrace	1,400.17	16,481.00	36.00	281
Total (incl. open space)	8,906.84	58,936.00	1,888.00	301

TM Area Exclusions: entrance lobby area, penthouse rooms, common area (basement level for balconies), elevator shafts, mechanical / service rooms, storage spaces, parking & loading areas.

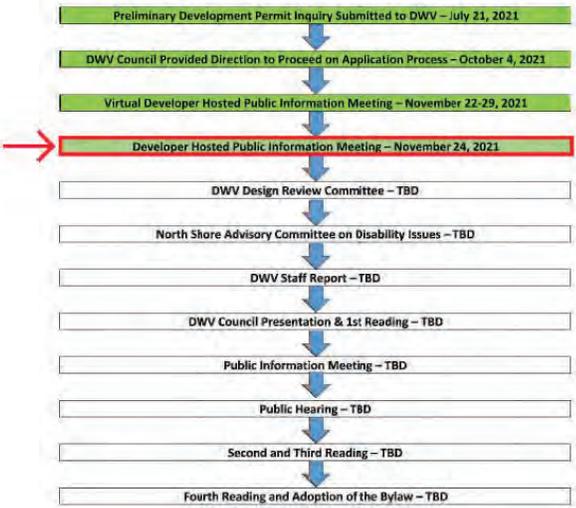
	Single Units	# of Units	Required	Provided
Residential Parking	0.00	0	0	0
Res. Visitor Parking	0.00	0	0	0
Total	0.00	0	0	0

	Single Units	# of Units	Required	Provided
Micro-unit Storage	0.00	0	0	0
Storage Spaces	0.00	0	0	0
Total	0.00	0	0	0

Notes: 1.0 daily per unit required per bylaw for apartment residential.

Unit Mix	Type A (Micro-unit) (400 sq ft)	Type B (Micro-unit) (450 sq ft)	Type C (Apartment) (600 sq ft)	Type D (Apartment) (650 sq ft)	Type E (Apartment) (700 sq ft)	Total
Level 1	24	1	1	0	0	26
Level 2	24	1	1	0	0	26
Level 3	24	1	1	0	0	26
Level 4	24	1	1	0	0	26
Level 5	24	1	1	0	0	26
Level 6	24	1	1	0	0	26
Level 7	24	1	1	0	0	26
Level 8	24	1	1	0	0	26
Total	192	8	8	0	0	208
Bylaw	172	0	18	0	0	190
Total Micro-unit	172	Total Apartment	18	Total	208	
Total % Micro-unit	82%	Total % Apartment	8%			

## TIMELINE:



## SUMMARY

### Clyde Avenue Microunits Executive Summary

This proposal presented for the consideration of a Development Permit and Rezoning submission responds to the need for more diverse and affordable rental housing in West Vancouver. While the notion of for rent micro-units is new to West Vancouver, this type of housing has provided comfortably smart living, at an attainable price range, to young professionals, frontline and service workers in communities around the world.

The introduction of micro-units is proposed in this particular location given the proximity to public and active transportation, as well as close proximity to amenities, shopping and services. It is a creative solution to help West Vancouver address some of the challenges outlined in the November 2020 West Vancouver Housing Report. The Report states in part "labour force participation rates have been on the decline, and close to three-quarters of the labour force **commutes** into the municipality for work. Feedback from engagement indicates **the cost of housing is increasingly out-of-reach and employers are struggling to recruit and retain staff. Mid-market rental and affordable home ownership should be strongly considered to help meet the needs of the local labour force.**"

The Clyde Avenue neighbourhood consists of market housing, rental housing, senior living, offices and services. The introduction of micro-unit rental accommodation is not only complimentary to the existing uses, but would also create an attractive entrance feature, as a gateway to the existing compact cluster of buildings.

Park Royal is excited about working with West Vancouver Council, staff and the community to bring this innovative and forward-looking style of rental accommodation to life.



671, 685, 693 CLYDE AVE & 694 DUCHESS AVE, WEST VANCOUVER | 2021-11-24

# PROPOSAL

## Existing & Future Context

The development site is located on Clyde Avenue off Taylor Way and is characterized by the following features:

- Comprised of three lots fronting Clyde Avenue, as well as a single lot fronting Duchess Avenue.
- Has never been developed and is currently serving as an ancillary parking lot for Park Royal North commercial.
- To the east is an existing 5-storey seniors' housing complex, to the south is an existing 6 level parking structure (34' 8" high)
- To the north is existing 2-storey single family residential housing.
- To the west is a northbound exit ramp that allows the vehicular traffic from Park Royal North to exit onto Taylor Way.
- The north edge of the site is characterized by a steep embankment with mature vegetation.
- The parking structure to the south is zoned for that use and currently there are no plans for redevelopment as it was excluded from the Marine Drive Local Area Plan.
- Further north, the 657 Marine Dr. development has been approved at Marine Dr. & Taylor Way intersection including buildings from 4-8 storeys high.

## Project Description

The proposal conforms to the District's current OCP as it relates to Clyde Avenue. However, given the current zoning the property needs to be rezoned in order to allow construction of 100% Rental Micro-unit Housing as follows:

- Rental Micro-unit Housing development at the corner of Clyde Ave. and Taylor Way
- 6-storey housing component with rooftop amenity
- FAR: 2.00 (1.75 base + 0.25 Rental bonus)
- FAR Floor Area: 8,113 sm (87,328 sf)

- Unit Type: Rental Micro Units
- Number of Units: 201
- Unit Mix:
  - 87% Move-in Ready Micro-units (350 sf)
  - 13% Adaptable Level 2 Micro-units (470 sf)
- Parking: 51 stalls at 0.25 stalls per unit
- Bicycle Parking: 301 stalls at 1.5 stall per unit

## Project Design Rationale

The proposed development is located in the transition area between the 4-8 storey density at Marine Drive and the 2-storey single family neighbourhood to the north. The project design is based on the following major considerations:

- 6-storey building massing mediates between the larger scale to the south and the lower scale to the north, as well as the 5-storey seniors housing to the east.
- The building is sited on the three lots fronting Clyde Avenue at the entrance to this neighbourhood.
- The northern lot off Duchess Avenue will not be developed as part of this proposal and provides an opportunity for outdoor green space or another amenity accessible to the public.
- The U-shape building massing configuration creates a south-facing courtyard that is buffered from the traffic at Taylor Way.
- The rental housing comprises a 6-storey massing block with a rooftop amenity garden.
- The project will provide 201 micro-units ranging between 350-478 sf in size, with all the units being move-in ready.
- The outdoor amenities will include the rooftop garden, as well as the south-facing courtyard green space with outdoor fitness area.
- Adjacent to the lobby at L1 over 2000 sf of indoor amenities will include a shared indoor working / IT spaces and dining / kitchen amenity.

- At each level from Level 2 to Level 6 there will be a lounge / coworking area off the elevator lobby.
- All vehicular and bicycle parking, as well as the garbage / recycling facilities are provided on a single level of underground parking at P1
- The bicycle facilities will include a bicycle repair area.
- Provisions for vehicular and bicycle electrical charging will also be included.
- The development will increase the supply of affordable rental housing in the area through provision of move-in ready micro-units.
- Located in close proximity to the Marine Drive / Taylor Way node, the residents will have access to public transportation, as well as active transportation modes.

## Compliance with OCP policies

The proposed development adheres to the intent of the OCP at Clyde Avenue East of Taylor Way (BF-D 2) with the following features:

- Provision of move-in ready rental micro-unit housing with close proximity to transit / active transportation
- High performance building with Energy Step Code: Step 3 with LCES or Step 4 compliance
- Electrical charging for cars and bicycles
- Extensive indoor / outdoor amenities
- Provision of public green space at Taylor Way / Duchess
- High quality windows to mitigate traffic noise from Taylor Way
- Utilizing reclaimed wood from the original 1950 Park Royal North buildout
- Landscaped courtyard with pedestrian-friendly design features at Clyde Avenue
- Articulated building facades to better relate to the existing 2-5 storey neighbourhood scale
- Gateway articulation at Clyde Ave / Taylor Way through prominent, highly-articulated stair towers



## EXISTING CONTEXT: NEIGHBOURHOOD



671, 685, 693 CLYDE AVE & 694 DUCHESS AVE, WEST VANCOUVER | 2021-11-24

## EXISTING CONTEXT: DEVELOPMENT



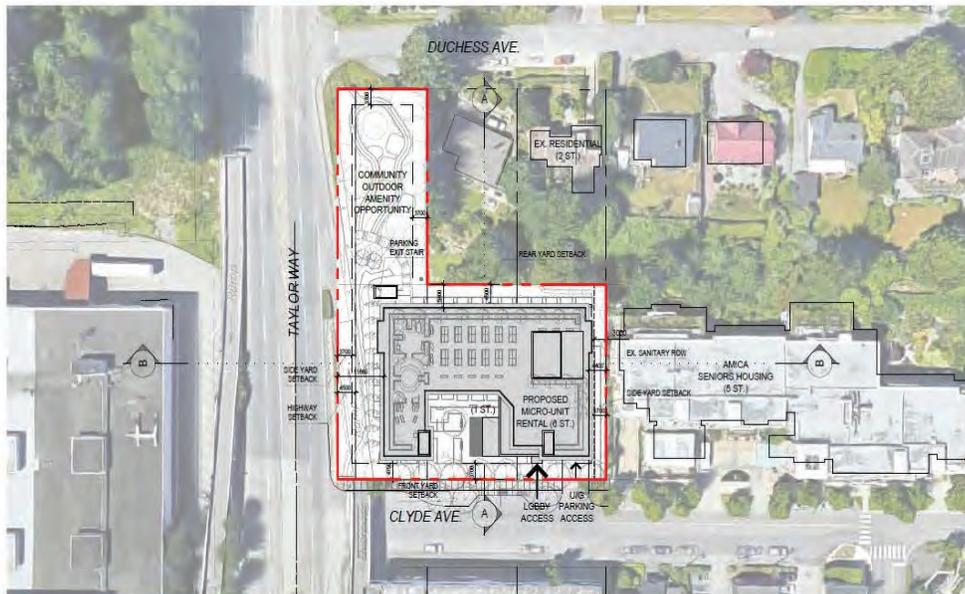
671, 685, 693 CLYDE AVE & 694 DUCHESS AVE, WEST VANCOUVER | 2021-11-24

## EXISTING CONTEXT: PHOTOS



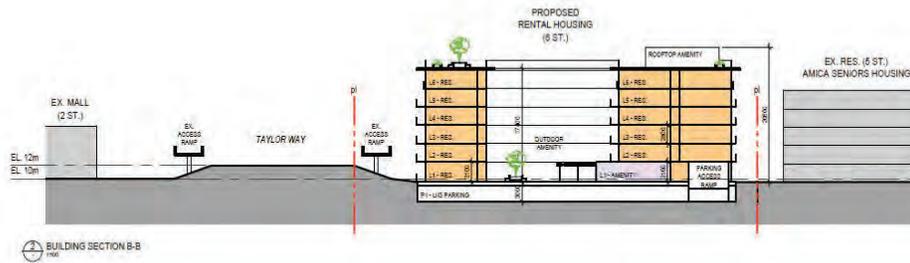
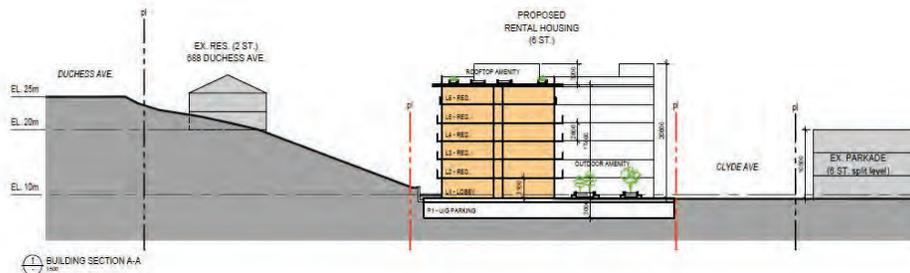
671, 685, 693 CLYDE AVE & 694 DUCHESS AVE, WEST VANCOUVER | 2021-11-24

# SITE PLAN



671, 685, 693 CLYDE AVE & 694 DUCHESS AVE, WEST VANCOUVER | 2021-11-24

# SITE SECTIONS

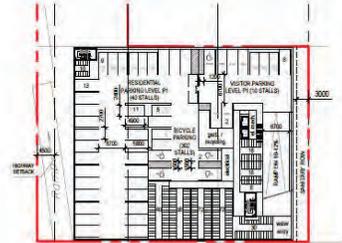


671, 685, 693 CLYDE AVE & 694 DUCHESS AVE, WEST VANCOUVER | 2021-11-24

# FLOOR PLANS



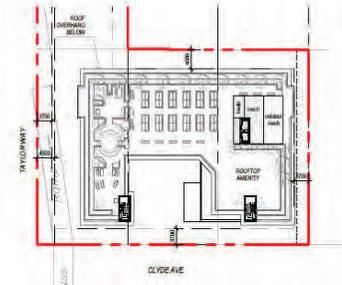
1 LEVEL 1 FLOOR PLAN



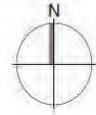
4 PARKING LEVEL P1



2 LEVELS 2-6 FLOOR PLAN



3 LEVEL 7 FLOOR PLAN



- NOTE:**
- 100% parking stalls with EV charging rough-in provided
  - hatched bicycle parking stalls are stacked (22%)

LEGEND	
Unit Type A:	Micro Unit 32.51 sm (350 sf)
Unit Type B:	Micro Unit 35.28 sm (379 sf)
Unit Type C:	Adaptable* Micro Unit 44.42 sm (478 sf)
Unit Type D:	Adaptable* Micro Unit 43.6 sm (469 sf)
Unit Type E:	Adaptable* Micro Unit 40.45 sm (435 sf)

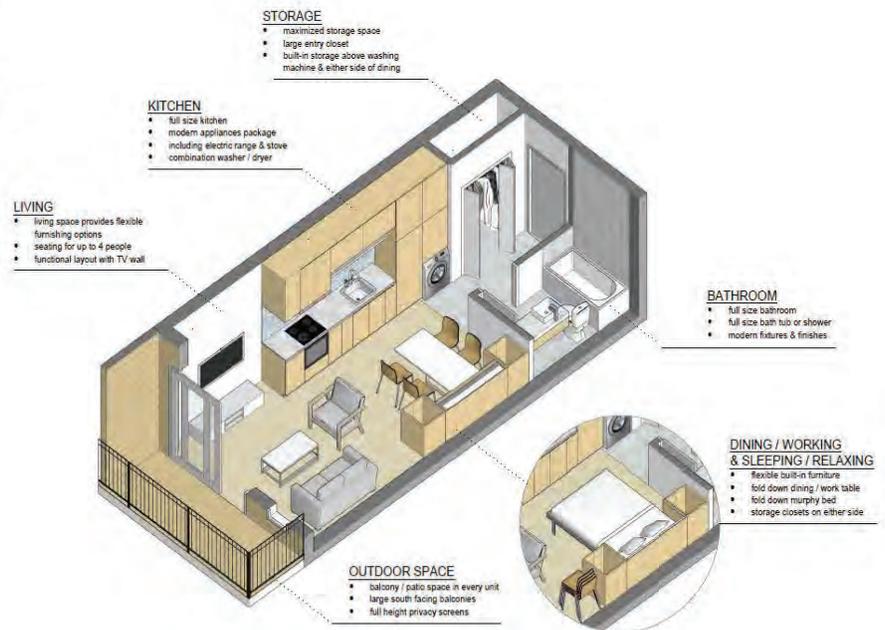
\*All adaptable units achieve Level 2 Adaptability Standard.



# MICRO UNIT CONCEPT

## TYPICAL MICRO UNIT

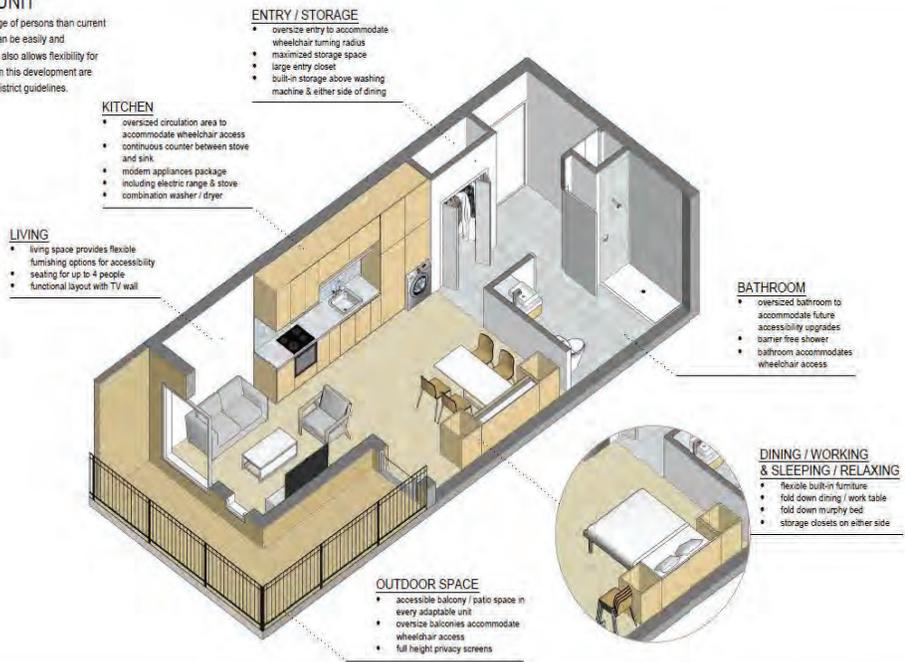
- 82% of Micro Units at 350sf provide an affordable rental price point for the residents
- Flexible space provides for all the necessities of living, dining, sleeping, relaxing and entertaining
- In-suite laundry with combination washer & dryer in each unit
- Contemporary interior design in the kitchens and bathrooms with energy efficient appliances
- High performance building with increased thermal comfort
- The amenity package is designed to complement the efficient suite layouts with over 2,000 sf of the indoor amenity area, as well as the expansive rooftop outdoor amenity garden



# MICRO UNIT CONCEPT

## TYPICAL ADAPTABLE MICRO UNIT

Adaptable design creates liveable residences for a wider range of persons than current building codes require. By considering design features that can be easily and inexpensively incorporated at a future time, adaptable design also allows flexibility for residents whose needs may change over time. 13% of units in this development are designed to meet the Level 2 Adaptability Standard per the District guidelines.



671, 685, 693 CLYDE AVE & 694 DUCHESS AVE, WEST VANCOUVER | 2021-11-24

# TARGET MARKET: Workplace & Young Professionals Housing

Medical Support Staff

Service Workers

Tech workers

Library workers

Retail workers

Film workers

ECE workers

Teachers

Senior care

Anyone who works long hours

# 3D VIEWS



1 NORTH-WEST CORNER - EXISTING CONTEXT



2 NORTH-WEST CORNER - FUTURE CONTEXT



3 SOUTH-WEST CORNER - EXISTING CONTEXT



4 SOUTH-WEST CORNER - FUTURE CONTEXT



671, 685, 693 CLYDE AVE & 694 DUCHESS AVE, WEST VANCOUVER | 2021-11-24

## 3D VIEWS



⊕ WEST ELEVATION - TAYLOR WAY



⊕ NORTH ELEVATION



⊕ SOUTH ELEVATION

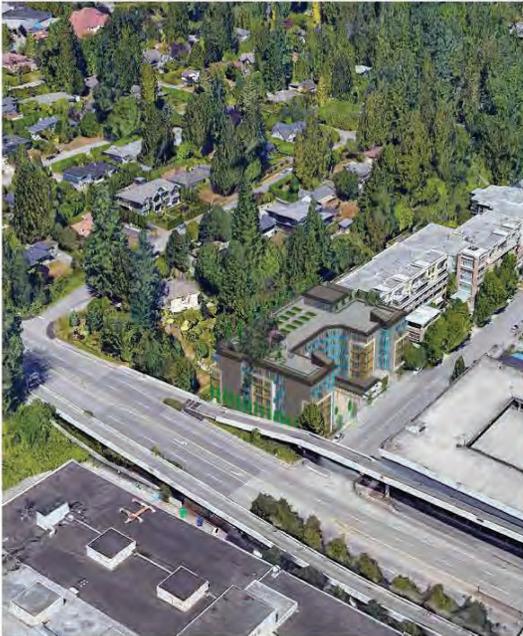


⊕ SOUTH / EAST ELEVATION



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## 3D VIEWS



⊕ AERIAL VIEW - LOOKING NORTH-EAST



⊕ AERIAL VIEW - LOOKING SOUTH-WEST



671, 685, 693 CLYDE AVE & 694 DUCHESS AVE, WEST VANCOUVER | 2021-11-24

# SUSTAINABILITY



## Enhancing Resiliency

- Shading of site hardscapes to reduce heat island effect, including green roof amenity space and landscaped central courtyard
- Potable water use reductions through installation of native landscape species and low flow plumbing fixtures to prepare for water scarcity through summer drought
- 'Park-like' public amenity and urban agriculture amenity will provide some residents the opportunity to grow their own food improving social resilience of the community
- Design for improved thermal comfort through optimized window to wall ratio, high performance glazing and exterior shading

## BUILDING SUSTAINABILITY

### Energy Performance: Step 3 with a Low Carbon Energy System

- High performance building envelope reducing heating and cooling energy consumption
- Optimized glazing placement balancing daylight penetration and thermal comfort
- Reduced reliance on fossil fuel combustion for space heating and domestic hot water with low carbon heat pump based systems

### Low Carbon Construction

- Wood frame construction supports the local low carbon building material industry, while reducing the project's embodied emissions

### Low Carbon Transportation

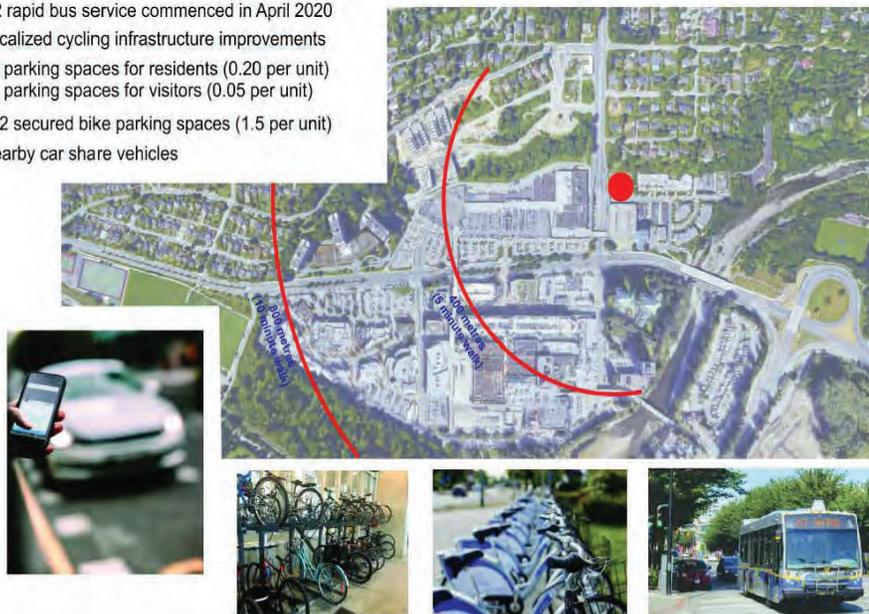
- Car parking will meet the minimum provision and will be equipped to support level 2 electric vehicle charging to support the transition away from fossil fuel vehicles
- Bicycle parking and a bicycle repair station will support sustainable living and further support the mode shift away from vehicles for the dense city living lifestyle



671, 685, 693 CLYDE AVE & 694 DUCHESS AVE, WEST VANCOUVER | 2021-11-24

## TRANSPORTATION - OPTIONS

- Multiple public transit routes with nearby stops
- R2 rapid bus service commenced in April 2020
- Localized cycling infrastructure improvements
- 40 parking spaces for residents (0.20 per unit)  
10 parking spaces for visitors (0.05 per unit)
- 302 secured bike parking spaces (1.5 per unit)
- Nearby car share vehicles



# TRANSPORTATION - TRAFFIC



Existing Peak Hour Traffic Volumes



**Proposed Development - Site Traffic**

**Weekday Morning and Afternoon Peak Hour**  
 20 resident vehicles  
 20 other (i.e., ride-hailing vehicles, taxis)



# LANDSCAPE

## POTENTIAL EXERCISE EQUIPMENT FOR FITNESS/NATURE TRAIL



## EXAMPLES OF POTENTIAL PLANT CHOICES



### TREES



### NATIVE SHRUBS AND GROUNDCOVERS



### GARDEN PLANTS FOR PATIOS AND DECKS



### DROUGHT TOLERANT PLANTS AND GRASSES

## Appendix E: Comment Form

# Proposed Rezoning: 671, 685, 693 Clyde Avenue and 694 Duchess Avenue Micro Unit Rentals

*Public Information Meeting - Hosted by Park Royal – November 24, 2021*

### Comment Form

Thank you for attending our Public Open House, we trust the information provided was helpful. The following is a brief summary of our preliminary proposal:

- 201 Residential Apartment Micro Units. (2.0 FAR)
- Parking stalls at .25 stalls per unit (approximately 51 stalls)
- Bicycle storage 1.5 per unit (approximately 302)
- 6 story residential building with landscaped rooftop amenity
- Northern lot (694 Duchess Avenue) outdoor green space or other amenity

Please provide your comments below and on the other side of this sheet. You may place your comments in the “comment box” provided.

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DATE:

RESPONDENT'S

ZIP CODE:

NAME:

EMAIL:

ADDRESS (Optional):

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## Appendix F: In Person PIM Comment Form Transcriptions

<p>Very much need affordable rentals, and I work @ care facility-+allow pets! <b>Amica Employee and North Vancouver resident</b></p>
<p>Great to see this kind of “affordable density” come to W Van. Need more of these kinds of projects in W. Van. It will get/retain a younger demographic resident in the community <b>West Vancouver resident</b></p>
<p>STRONGLY IN SUPPORT OF THIS PROPOSAL AS IT MEETS SO MANY OF THE NEEDS OF WV <b>West Vancouver resident</b></p>
<p>I’m in support <b>West Vancouver resident</b></p>
<p>A creative concept that will meet a definite need in our community-the utilization of a property that has been an unfortunate “eyesore” entrance to our community <b>West Vancouver residents</b></p>
<p>Fascinating Concept and Idea. Full rentals in the centre of transportation connection in West Van. Ticks almost every box for affordability and need <b>West Vancouver resident</b></p>
<p>I like the proposal. I hope it succeeds. We need “employment” housing on the North Shore otherwise it will become a disfunctional community <b>North Vancouver resident</b></p>
<p>The 100% rental, affordable, bikeable characters of this project along with the guidelines stated in the OCP. The close proximity to the bridge puts less pressure on the existing traffic congestion. The abundant green space provides buffer <b>West Vancouver resident</b></p>
<p>This is an excellent idea! It would definitely help save time by not having to transit half the day. Also it would to have a place on their own to get some down time to relax alone. It would definitely help save money by being so close to work, travel, food, and getting all this furniture, house stuff <b>North Vancouver resident</b></p>

## Appendix G: Virtual PIM Comments

<p><b><i>Are there any other potential opportunities, benefits or concerns you think should be considered?</i></b></p>
<p><b><i>Open-Ended Response</i></b></p>
<p>In addition to the pedestrian link along Taylor Way between Marine Drive and Duchess Ave, I would like to see room made for a bike lane, preferably protected from pedestrians and motorists to connect to the planned separated bike lane on the north side of Marine Drive to Taylor Way and partway up Taylor Way towards Clyde Ave. <b>West Vancouver resident</b></p>
<p>Unit priority registration focused on workers with full time schedules - suggest there may be an opportunity for part-time workers such as students. Would agree with the low carbon energy system - a must. Love the pedestrian/cycling improvements. Vehicle traffic may be a concern - would appreciate more staff assessment on this. Privacy of neighbours in behind may be a concern - especially with roof-top activity. Interesting use of an unusual lot - love the rental micro-units offered to help meet our housing needs. <b>West Vancouver resident</b></p>
<p>I fully support this initiative. I think we need a diversity of housing and it needs to be affordable. Just a few thoughts: I hope a foot bridge or tunnel will allow pedestrians and cyclists safe and quick connection between the site and a westerly and southerly direction. Although there are traffic lights there, in the dark, on a wet night, it would be safer to not have to negotiate the crossing particularly from that east corner of Taylor/Marine, across the feeder lane from Marine to Taylor. <b>West Vancouver resident</b></p>
<p>I've read your FAQs and you seem to have covered all the bases. Just want to voice my support for this initiative. <b>West Vancouver resident</b></p>
<p>I want to voice my support for this residential concept to diversify the tenure and pricing options for our local workforce and to implement some or the innovations reflected in terms of transportation modes and on-site recreation amenities. I would like to see similar innovation locally for affordable ownership opportunities, too. <b>West Vancouver resident</b></p>
<p>WE are totally on board with this affordable proposal for reasonable housing. As a long time resident the North shore I feel it is important that we house in place people who work in the community. We also hope that this sets a precedent for further developments that provide affordable accommodation for the average working class. We are also pleased that locals are to be given priority. <b>North Vancouver resident</b></p>

Love the idea of an outdoor pickleball court! Great for fresh air and exercise at any age. Thank you!

Hi there, I like this proposal and support it, provided that a workable system is set up and maintained, which steers people who work in the essential services sector in West Vancouver, into these rental units. Living successfully in such a tiny space turns on storage. For example, where do you hang your wet towel/pj's/bathrobe? Make sure each bathroom door has two good quality hooks on the back of it to supplement towel/pyjama/bathrobe storage. Ideally, these units would have below counter refrigerators which are so common in the UK and Europe. It lowers your hydro bill, and creates more precious storage. It also turns on effective public spaces, like those for garbage and recycling. In such a tiny space, there won't be enough room to keep hardly any garbage or recycling, plus food waste will smell up such a small space quickly, so the public garbage/recycling area should be pleasant, easily accessible, well lit, safe and with maybe a clean surface to set stuff down and sort it out, with the same upbeat feel as the bike repair area. It should contemplate continuing changes to the municipal recycling regime. Right now, for example, there's no glass collection. It should take batteries, lightbulbs, broken glass, electronics etc. They can take their unused pharmaceuticals back to Shoppers. Residents won't have cars to go to a recycling depot. Today, garbage and recycling has to be right up front, not around the back where it's not kept tidy. Maybe the building could somehow specifically accommodate the new Buy nothing movement with a place for residents to swap their unwanted belongings. You might offer to support residents with the interior decorating by giving them standard sizes for the couch, lounge chair, coffee table, tv screen, and patio furniture. Also, for example, what size of dishes will fit in the cupboards, cookie sheet or muffin tin will fit in the oven, and kitchen garbage container will fit under the sink. Finally, I don't think you mentioned the proximity to the trails up the Capilano Canyon and down to Ambleside Park, the Seawalk and the Millennium Trail which are all priceless amenities and part of the car-free lifestyle that the residents will enjoy. **Vancouver resident**

We are very supportive of this project. We had a chance to attend the open house on Wednesday Nov. 24th, and really enjoyed it. Great displays and the floor outlines were very helpful. Lots of people associated with the project on hand to answer detailed questions. This type of housing is needed throughout the North Shore. Hopefully, this will create a model that can be duplicated elsewhere. All in all, we love it; fabulous idea! Opportunities: - Good location, opportunity to duplicate the project in the adjacent area. - While we love gardening, the boxes take up a large part of the rooftop amenity space and can be relatively lifeless (unattractive) for parts of the year. They may be better located on the west side (adjacent to Taylor Way) and allow for the quieter center of the roof for the public space. Benefits: - Access to transit and downtown - Great access to all shopping needs and Ambleside park - Bike access to Stanley park etc. Concerns: - Limited parking (25% of residences). How will these be prioritized? - Traffic noise on the West (Taylor Way) side.

**North Vancouver resident**

We respectfully submit the following feedback for your consideration and response: - Zoning: The lots under consideration fall under the Upper Taylor Way Corridor planning area, and not the Marine Drive/Taylor Way Local Area Plan (see westvancouver.ca planning documents);<sup>[1]</sup><sub>[SEP]</sub> - Ownership/Community contribution: We believe the lots under consideration are owned by West Vancouver, with a 2021 assessed value of \$7.99M under current zoning (if zoned as the adjacent Amica facility, the land assessment would be closer to \$17M, based on 75% of their 2021 land assessment). This is a significant taxpayer contribution that warrants further consideration, and needs to be highlighted;<sup>[1]</sup><sub>[SEP]</sub> - Liveability: There is no mention of noise levels throughout the day/night at this location adjacent to a Provincial highway. Given the population density proposed, and the noise levels, how much time would residents be able to spend indoors or outdoors enjoyably? Would

<p>another location be better suited to this type of housing? (see: <a href="http://livablecities.org/articles/can-micro-units-be-livable">http://livablecities.org/articles/can-micro-units-be-livable</a>). Thank you, <b>s.22(1)</b></p> <p><b>Address Unknown</b></p>
<p><b>s.22(1)</b> Community Housing Action Committee who advocate for affordable housing on the North Shore. <b>s.22(1)</b> advocate strongly for affordability wherever we can find it-so I'm wondering what rents you expect for these micro suites <b>North Vancouver resident</b></p>
<p>As a micro unit dweller myself, the inclusion of laundry and fully equipped kitchens is a wonderful idea. I also back the idea of the green space on the roof and more room for bicycles, encouraging people out of their cars.</p>
<p>I am in principle in full support of the development as proposed, with a few additional amendments.</p> <ol style="list-style-type: none"> <li>1. on the rooftop : ensure elevator access to the gardens for handicapped people. (wheelchairs)</li> <li>2. Add to the rooftop structure a 16 ft full width bay to provide a multi purpose closed common space, where a noisy musician can practice cello or saxophone , and a portion to accommodate garden tool storage and picknick furniture</li> <li>3. At the garden level create some additional space to allow for a Tool Library or even a Sharing Depot, combined with a regular session of a repair cafe. You can google the successful 3 locations in Toronto to get a sense of the space requirements. These are very successful social incubators for the residents, in tune with the intent of your proposal <b>West Vancouver resident</b></li> </ol>
<p>I fully support this development. Especially important to me is the reduced use of fossil fuels in building and the low carbon use for renters. The information provided in this virtual developers meeting has been useful.</p> <p><b>West Vancouver resident</b></p>
<p>I support this development <b>West Vancouver resident</b></p>
<p>Great idea with lots of potential for those not wanting to have to share living accommodation with another person to manage the cost. With the great connections to Horseshoe Bay and the rest of the lower mainland this proposal would also make a great place to live for someone who already has a residence on Vancouver Island but works on the mainland. More communities and developers should embrace such housing options. <b>West Vancouver resident</b></p>
<p>I think this is a idea whose time has come. It is the right project in the right place and will provide some badly needed housing West Vancouver <b>West Vancouver resident</b></p>
<p>Greet idea. It's about time west van supported this. Great way to grow the city ! <b>West Vancouver resident</b></p>
<p>Given the small unit size, adding a workspace 'hub' for people and flex meeting/common space is excellent. This also has the potential to bring the community into the space. Charging stations for electric bikes and cars. Given growing concerns with climate change, making this a net zero carbon neutral demonstration building builds on the Park Royal brand and aligns with West Vancouver brand. <b>West Vancouver resident</b></p>
<p>All sound good. Have no problem with the idea. <b>West Vancouver resident</b></p>
<p>This is one of the most thorough presentations I have ever seen on a new development. It covers all of my questions.</p>
<ol style="list-style-type: none"> <li>1. Sound proof walls on the side where the senior home is.</li> <li>2. Numerous emergency response phones, since it is a complex, there will be many vulnerabilities in the premises from theft, fires and health scares.</li> <li>3. A bridge to access the mall</li> <li>4. For people that do drive, make a road mall side to access the complex.</li> <li>5. Many guest parking.</li> <li>6. Roof pool and hot tub.</li> <li>7. Easy accesses for emergency team</li> <li>8. Extremely powerful free wifi for the building.</li> <li>9. One or two indoor entertainment rooms with a screen and projector. <b>Burnaby resident</b></li> </ol>

These micro units are a very good idea! It's getting next to impossible to attract workers to West Vancouver because of high housing costs not to mention the cost and travel times associated with public transit! Matters will only get worse as times goes on. West Vancouver should support such a project as it's in the city's interest to do so. **North Vancouver resident**

Hi - I want to voice my support for this project. WVan desperately needs more affordable housing for workers within our community. This is also a great "first shot" for young people who want to access the outdoor amenities of the Northshore. I really like the outdoor fitness amenities (hope they come through with that). The development plans should include proper bicycle and pedestrian access to the surrounding area, which I don't necessarily see on the plans. I'd suggest a bike lane up from Marine along Taylor Way to at least Duchess. Thanks for your consideration. **West Vancouver resident**

As a longtime resident of West Vancouver (I moved here when I was s.22(1)). I would like to applaud Council for considering to green-light this initiative. This is a step in the right direction. We need to do more. We need larger housing options (not just micro-homes for 1 person) to accommodate varied family groupings. I have seen us lose amazing Teachers to districts and towns with more affordable housing that permit Teachers to start their own families-- we used to have so many Teachers with their own families here in West Van. I have seen seniors and young adults (long time residents of our communities) forced to leave their friends and families to more affordable districts. I have also heard many many shop keepers lament the fact that their is nobody here to work and keep their business afloat. What a privilege it is to live along side a wide section of communities, as I did when I was growing-up. What a privilege it is to be part of an inclusive community. I am in full support of this initiative and want to see more subsidized housing and affordable housing options in West Vancouver. **West Vancouver resident**

This is a great idea. Above is my contact information. I live at s.22(1) West Vancouver. **West Vancouver resident**